

STATE OF MAINE

STATE HIGHWAY COMMISSION



ROUTE 16

OVER

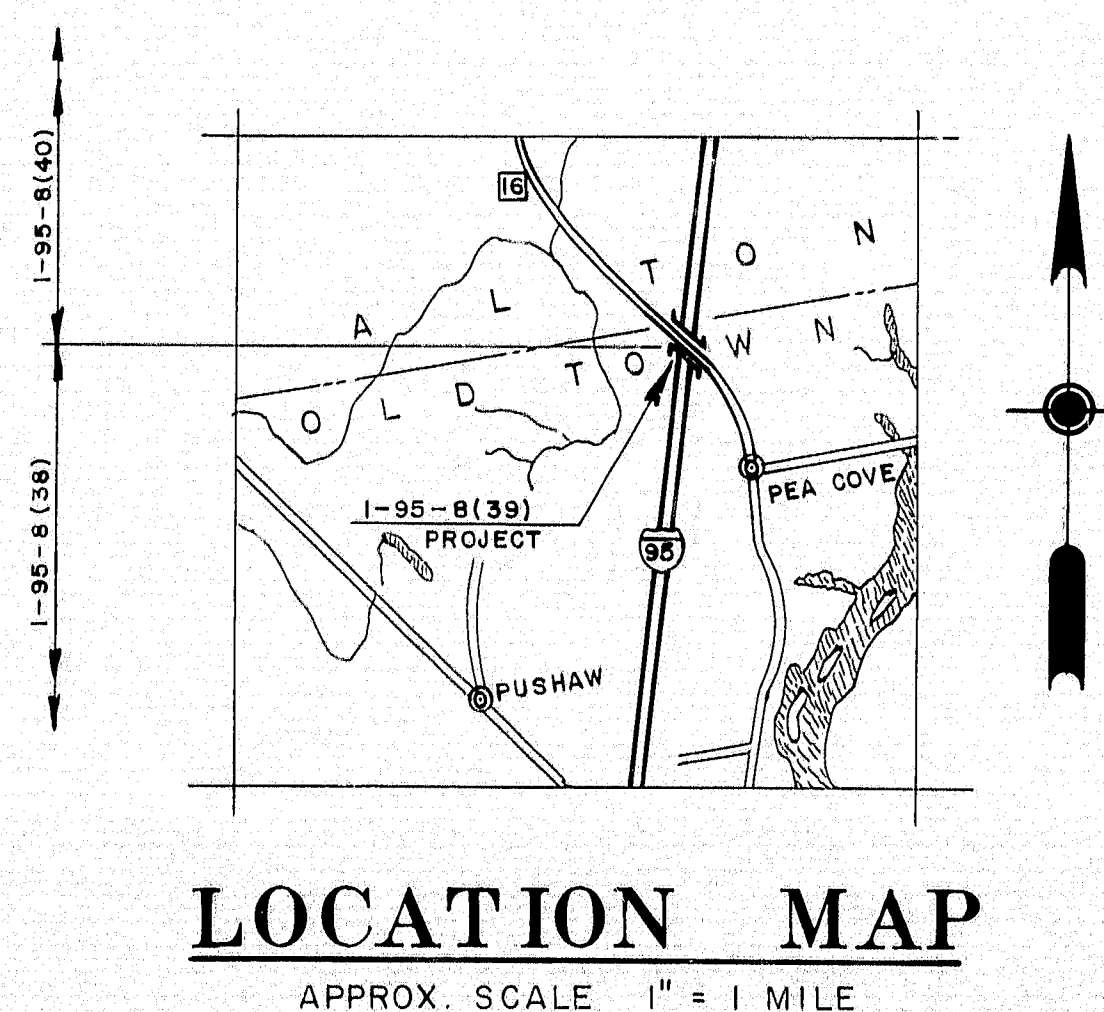
INTERSTATE 95

IN THE CITY OF

OLD TOWN

PENOBSCOT COUNTY

FEDERAL AID PROJECT NO. I-95-8(39)193



INDEX OF SHEETS

- 1 ----- TITLE SHEET
- 2 ----- GENERAL PLAN & ELEVATION
- 3 ----- FOUNDATION SURVEY
- 4 ----- BORING DETAILS
- 5 ----- ROADWAY WORK & SLOPE PROTECTION
- 6 ----- ABUTMENT NO. 1
- 7 ----- ABUTMENT NO. 2
- 8 ----- PIERS NO. 1, 2, 5 & 6
- 9 ----- PIERS NO. 3 & 4 - QUANTITIES
- 10 ----- STRUCTURAL STEEL - FRAMING PLAN
- 11 ----- STRUCTURAL STEEL - DETAILS
- 12 ----- BLOCKING
- 13 ----- SUPERSTRUCTURE - SPANS NO. 1 & 7
- 14 ----- SUPERSTRUCTURE - SPANS NO. 2 & 6
- 15 ----- SUPERSTRUCTURE - SPANS NO. 3, 4 & 5
- 16 ----- SUPERSTRUCTURE DETAILS
- 17 ----- REINFORCING STEEL

STANDARD DETAILS SHEETS

- BD 101-62 ----- BEARING DETAILS
- BD 102-62 ----- BRIDGE RAIL
- BD 103-62 ----- BEAM SPLICES
- BD 104-62 ----- DIAPHRAGMS, ARMORED JOINT,
SHEAR CONNECTORS, DRAIN
- 2-62 ----- GUARD RAIL, TYPE "E"

APPROVED
MAINE STATE HIGHWAY COMMISSION

Don H. Sturges
CHAIRMAN

Carl M. Stephens

Wangham
CHIEF ENGINEER

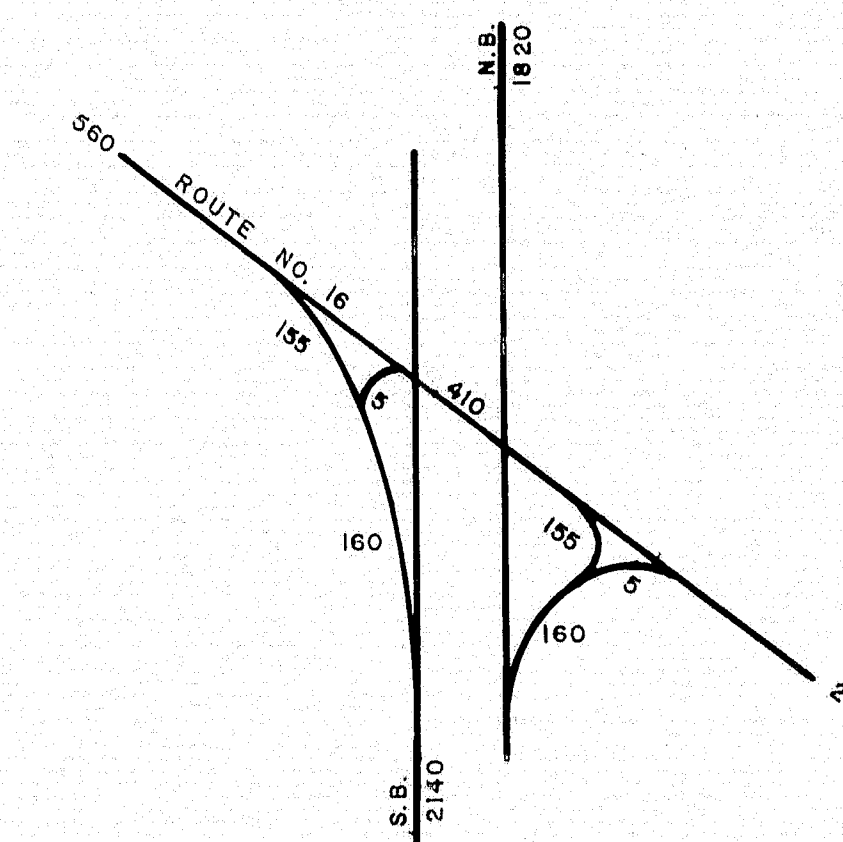
Feb. 5, 1964
DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS

REGION I

APPROVED

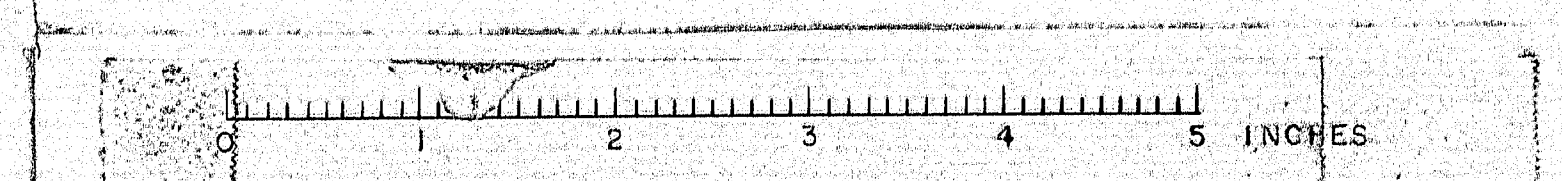
DIVISION ENGINEER DATE



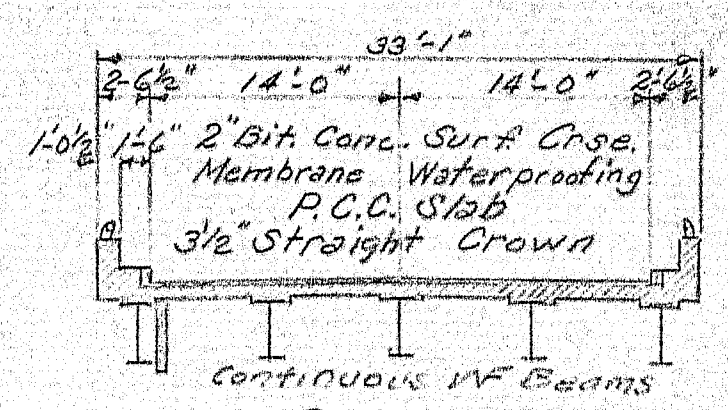
TRAFFIC

A.D.T. 1963 ----- 410
A.D.T. 1983 ----- 575
D.H.V. ----- 86
D ----- 70 %
T ----- 14 %
V ----- 60 MPH

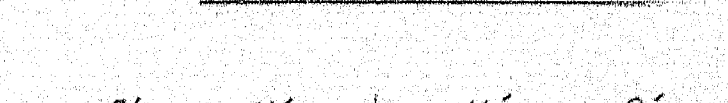
92-84



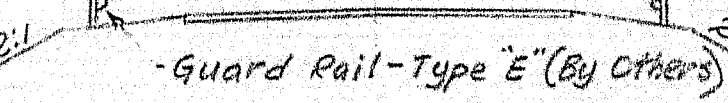
B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-8(39)		



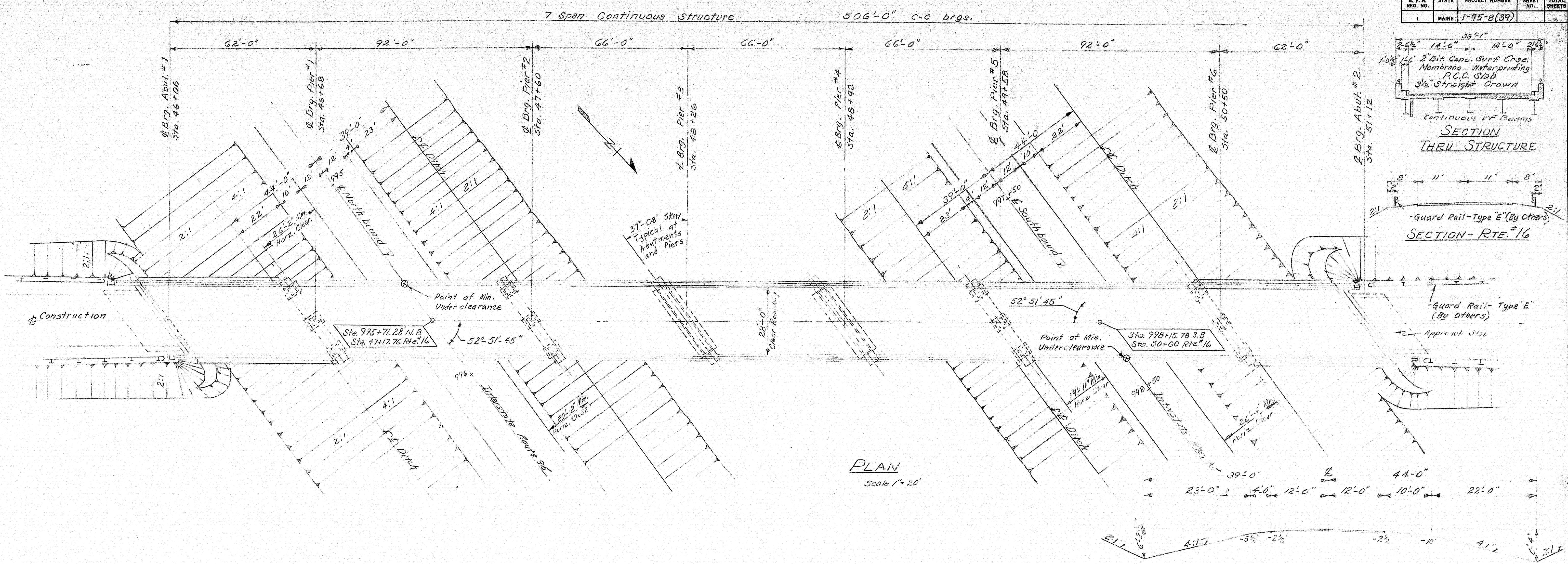
SECTION THRU STRUCTURE



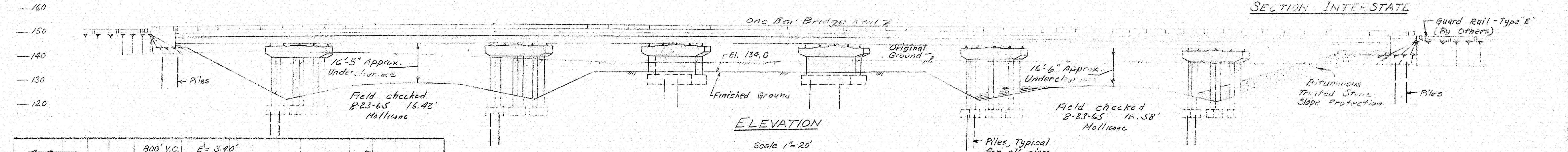
SECTION - RTE. #16



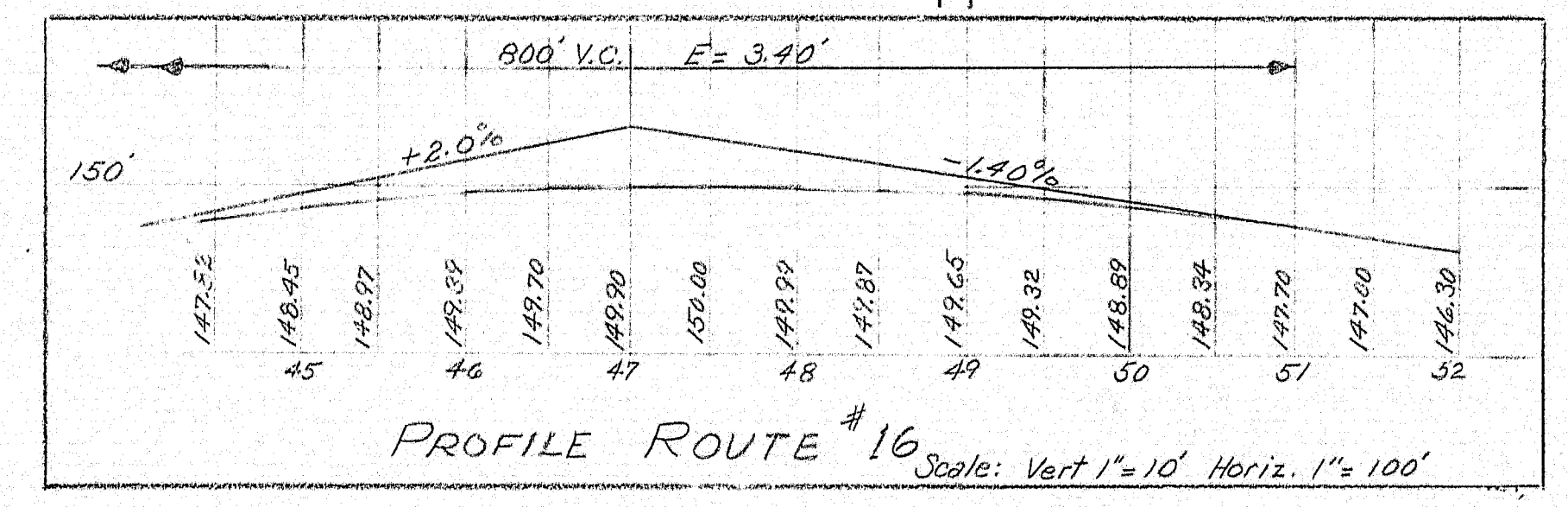
SECTION INTERSTATE



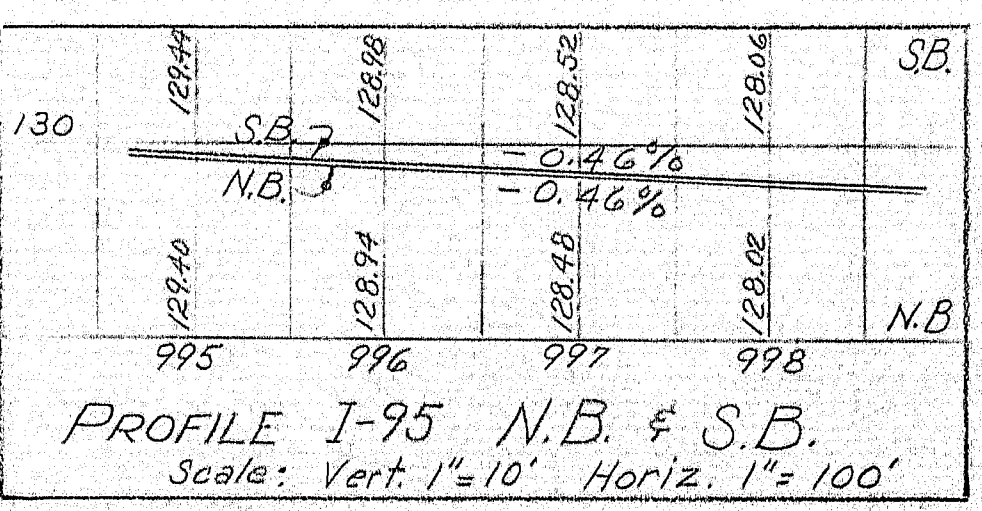
PLAN
Scale 1" = 20'



ELEVATION
Scale 1" = 20'



PROFILE ROUTE #16
Scale: Vert 1" = 10' Horiz. 1" = 100'



PROFILE I-95 N.B. & S.B.
Scale: Vert 1" = 10' Horiz. 1" = 100'

CONCRETE CLASSIFICATION
All concrete shall be Class "A"

STRUCTURAL STEEL CLASSIFICATION
All structural steel shall conform to the latest revision of A.S.T.M. Specification Designation A36 except as otherwise specified in the standard details.

ALLOWABLE STRESSES
Concrete - $f_c = 1200$ p.s.i., $n = 10$
Reinforcing Steel, Intermediate Grade - $f_s = 20,000$ p.s.i.
Structural Steel, A36 - $f_s = 20,000$ p.s.i.
Point Bearing Steel Piles - $f_s = 6000$ p.s.i.

SPECIFICATIONS
DESIGN: AASHO Standard Specifications for Highway Bridges 1961 with Interim Specifications 1961, 1962.
CONSTRUCTION: State of Maine, State Highway Commission, Standard Specs., Highways and Bridges Revision of January 1956 and Supplement Specifications February 1960.

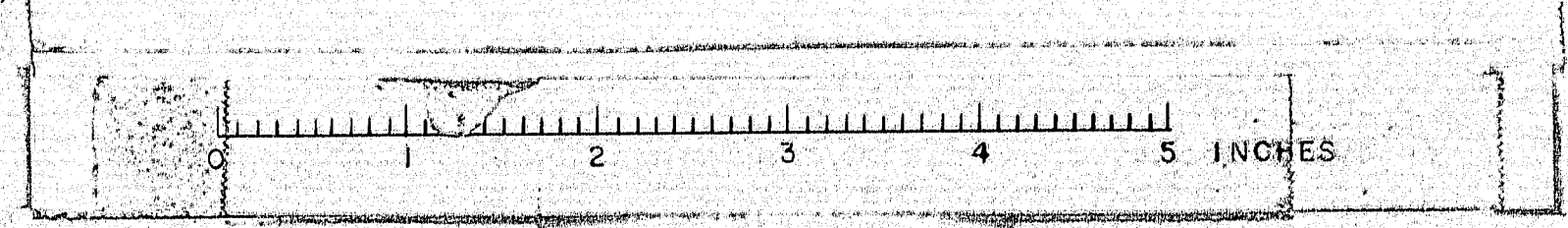
LOADING: H20-S16-44

BENCH MARK
Elevations are based on B.M. #38 which is located at Sta. 1000+00, off 250' left, spike in 30" Elm tree. Elevation is 139.25.

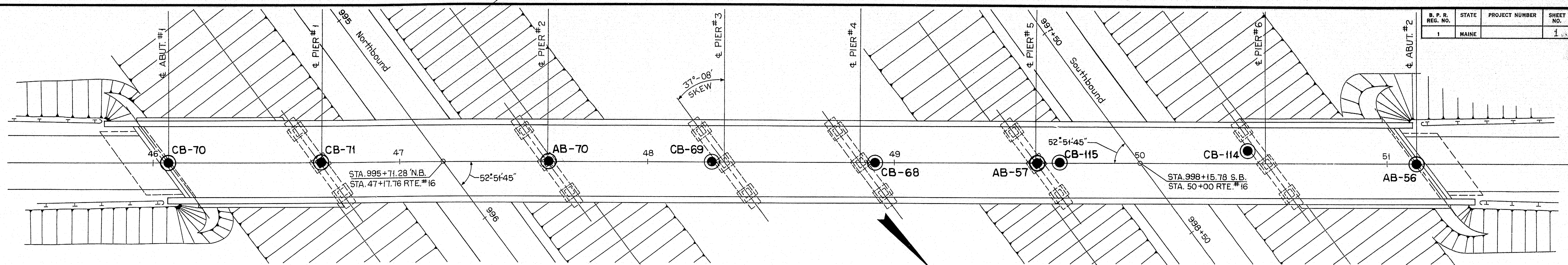
This plan shows ultimate construction for roadway work under this contract see sheet 5.

DESIGN - J.H.	BRIDGE NO. SURVEY - PLOT
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
ROUTE 16 OVER INTERSTATE 95 IN THE CITY OF OLD TOWN PENOBSCOT COUNTY	
GENERAL PLAN & ELEVATION	
SHEET 2 OF 17	AUGUSTA, MAINE JAN. 1964

92-85

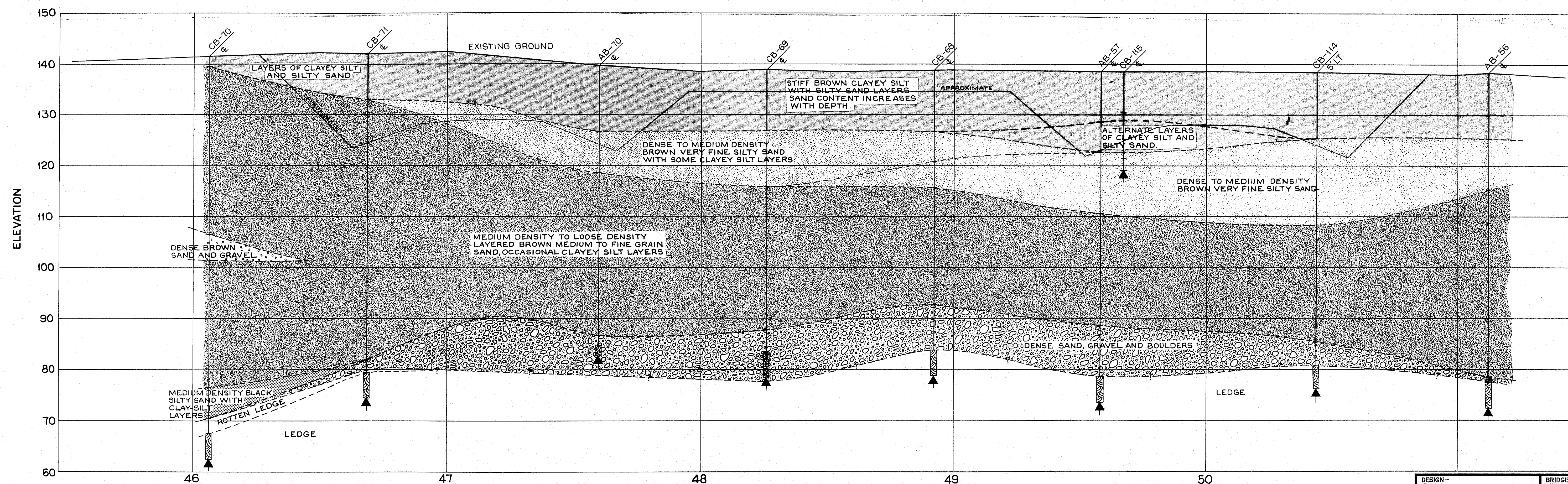


B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE		1	1



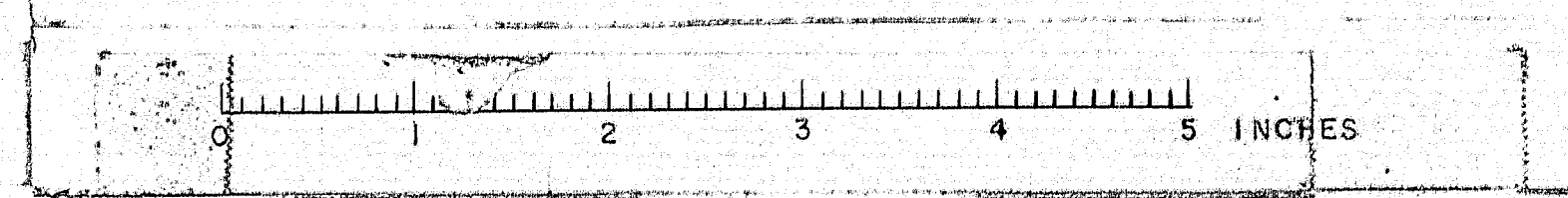
PLAN
SCALE : 1" = 20'

PROFILE
SCALE : 1" = 20' HORIZ.
1" = 10' VERT.

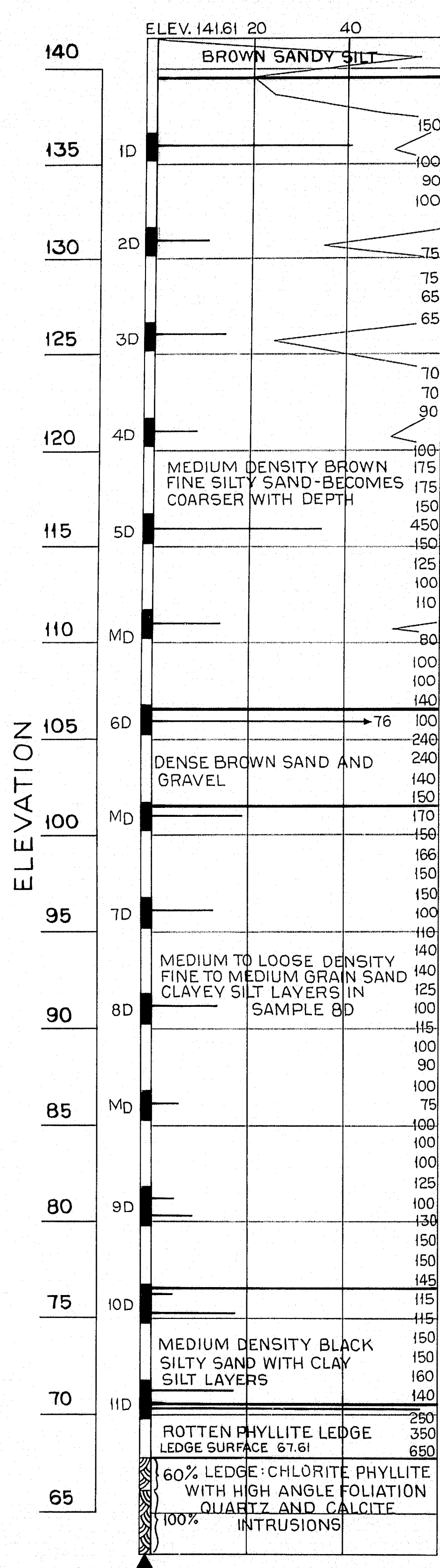


DESIGN— TRACE— CHECK—	BRIDGE NO. SURVEY— PLOT—
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
ROUTE 16 OVER INTERSTATE 95 IN THE CITY OF OLD TOWN PENOBSCOT COUNTY FOUNDATION SURVEY	
SHEET 3 OF 17 AUGUSTA, MAINE JAN. 1964	

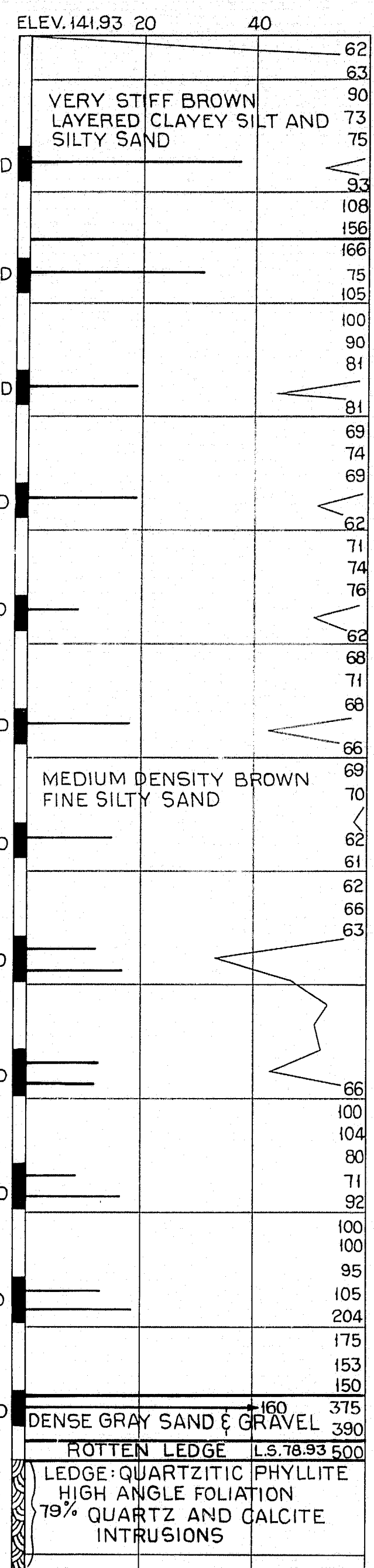
92-86



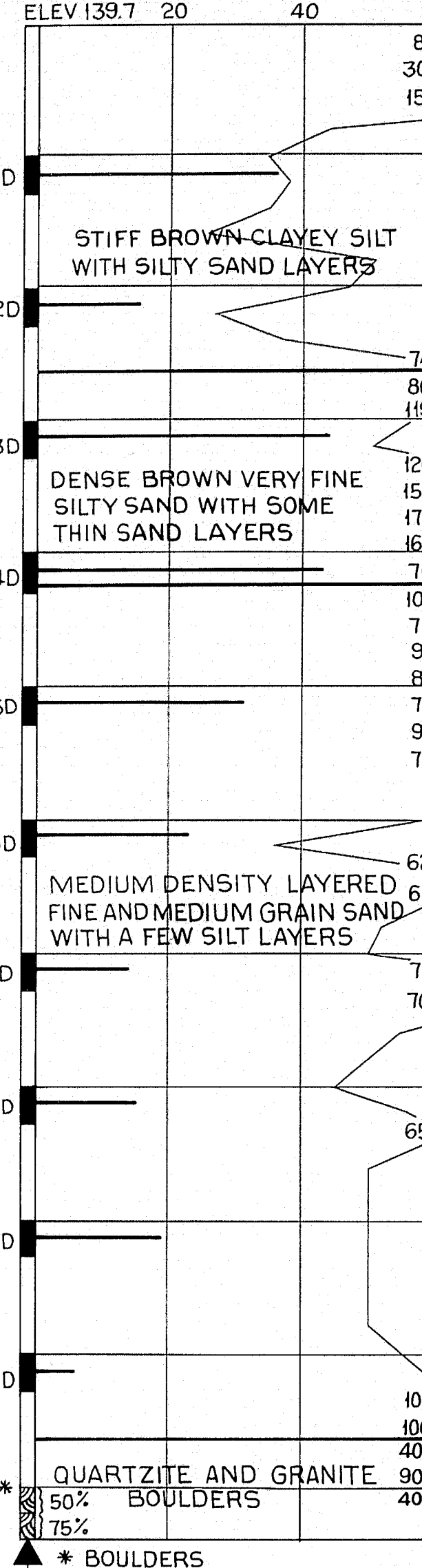
BORING CB-70 ABUT #1
STA. 46+06 & 2 1/2" CASING



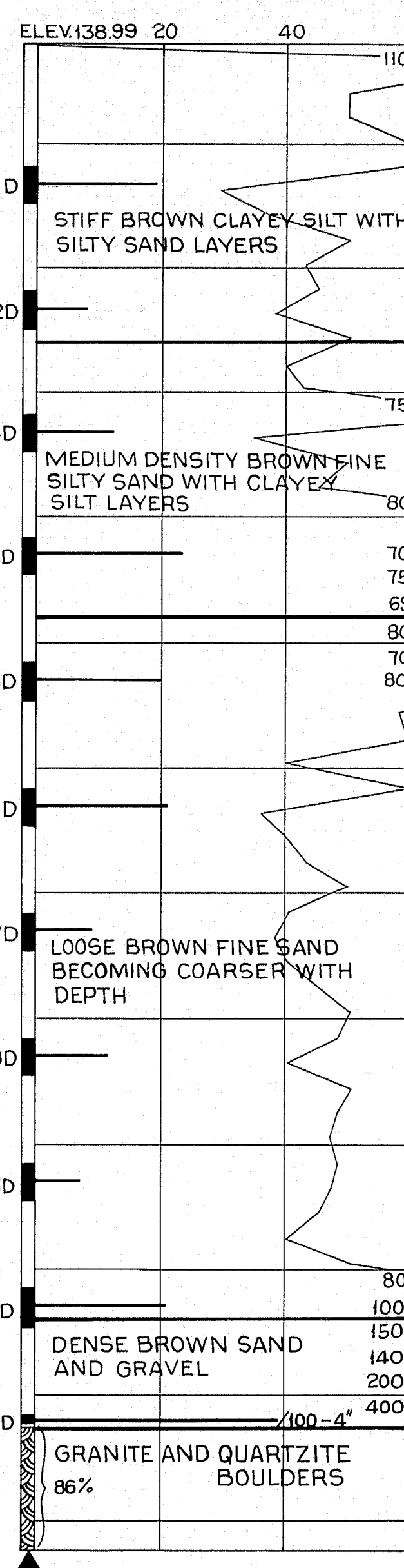
BORING CB-71 PIER #1
STA. 46+68 & 2 1/2" CASING



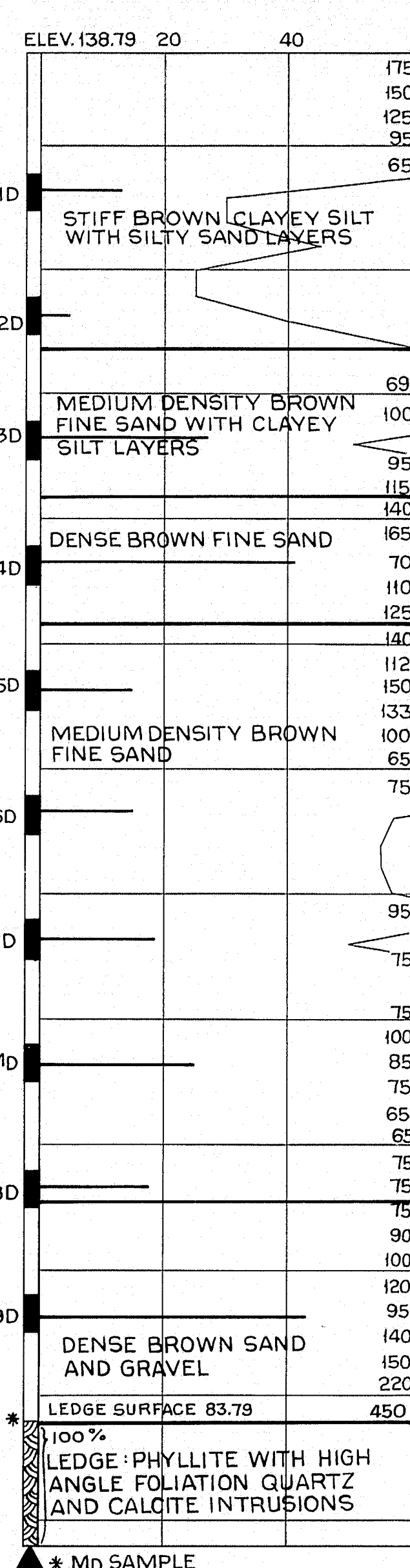
BORING AB-70 PIER #2
STA. 47+60 & 2 1/2" CASING



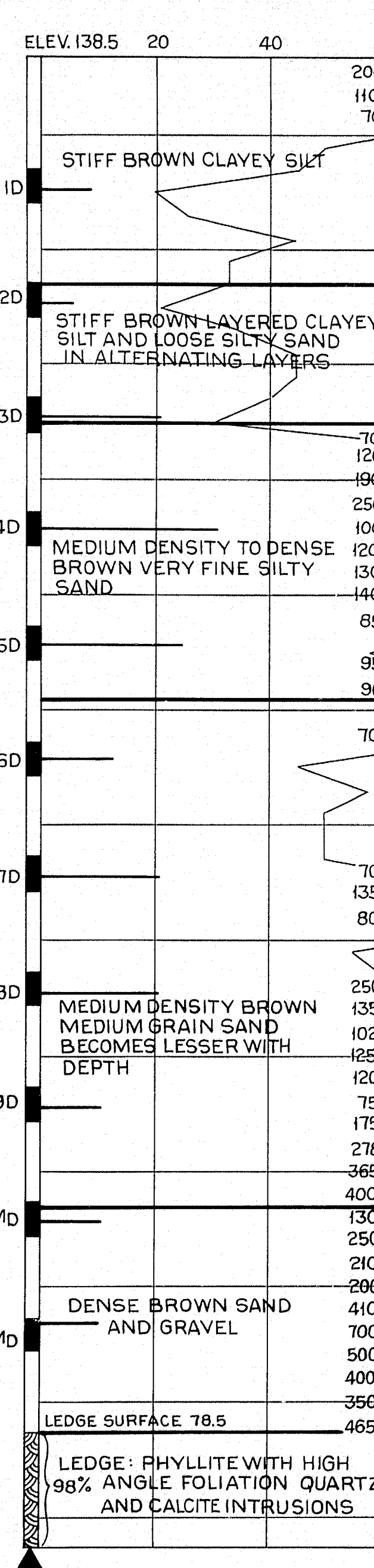
BORING CB-69 PIER #3
STA. 48+26 & 2 1/2" CASING



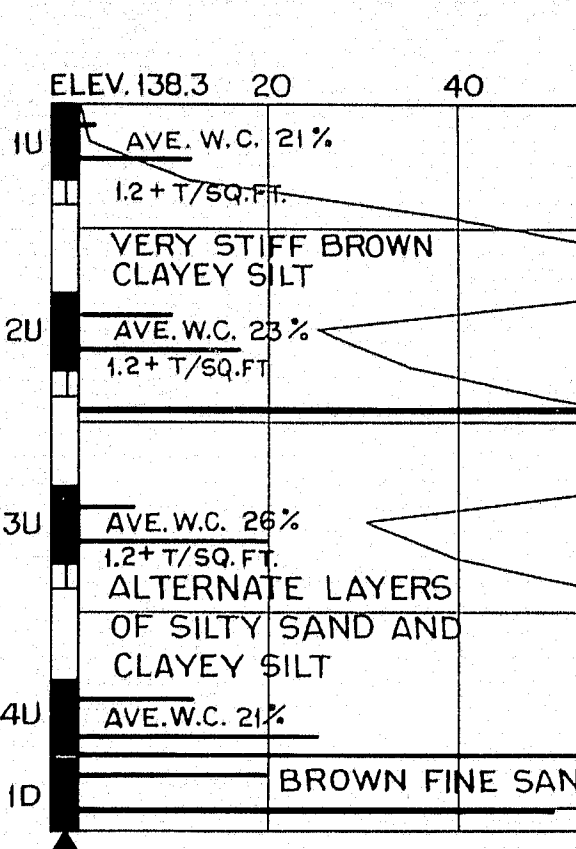
BORING CB-68 PIER #4
STA. 48+92 & 2 1/2" CASING



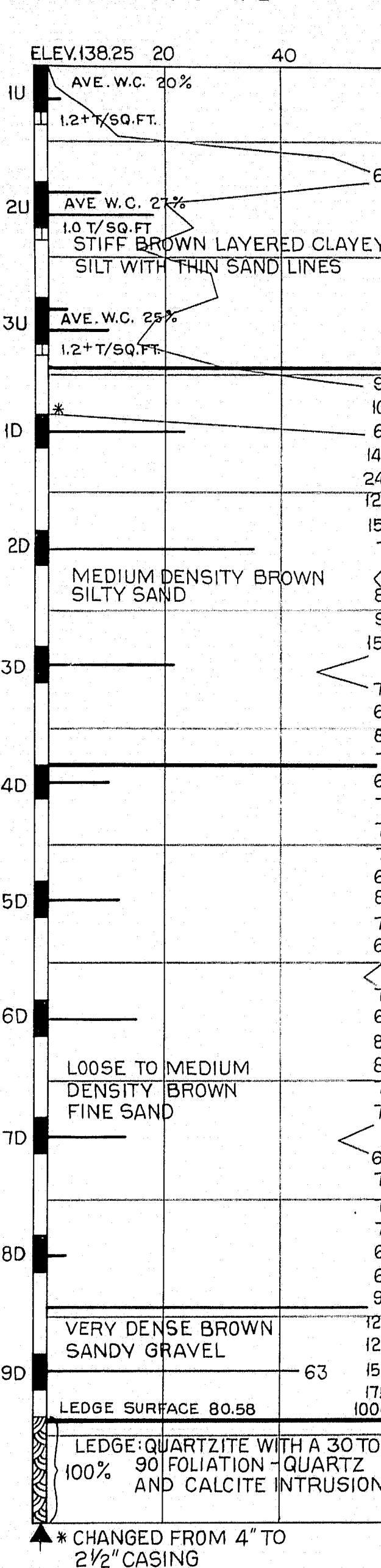
BORING AB-57 PIER #5
STA. 49+58 2 1/2" CASING



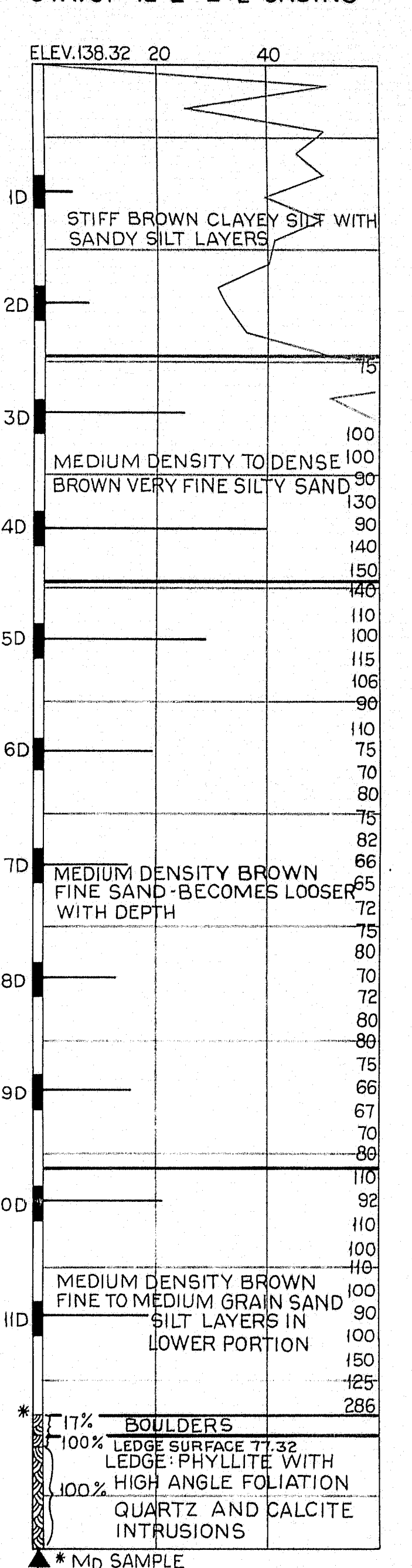
BORING CB-115
STA. 49+67 & 4" CASING



BORING CB-114 PIER #6
STA. 50+44 5' LT. &



BORING AB-56 ABUT. #2
STA. 51+12 & 2 1/2" CASING



BORING NOTES

- ALL SAMPLES AND VANES ARE MADE AHEAD OF CASING.
- NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 FT. LBS. OF ENERGY PER BLOW
- LOCATION OF SAMPLE OR SAMPLE ATTEMPT
- NUMBER AND TYPE OF DRY SAMPLE
- S & H SAMPLER #1290'S 1U 3 1/2" O.D. 16 GA. SEAMLESS TUBING
- MD UNSUCCESSFUL SAMPLE ATTEMPT AND TYPE OF SAMPLER
- NUMBER OF BLOWS REQUIRED TO DRIVE SPOON OR TUBING ONE FOOT WITH 350 FT. LBS. OF ENERGY PER BLOW
- FIELD VANE TEST
- ▲ BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA)
- ◆ LOCATIONS CORED BY DIAMOND BIT AND PERCENT RECOVERY OF ROCK

DESIGN -
TRACE -
CHECK -

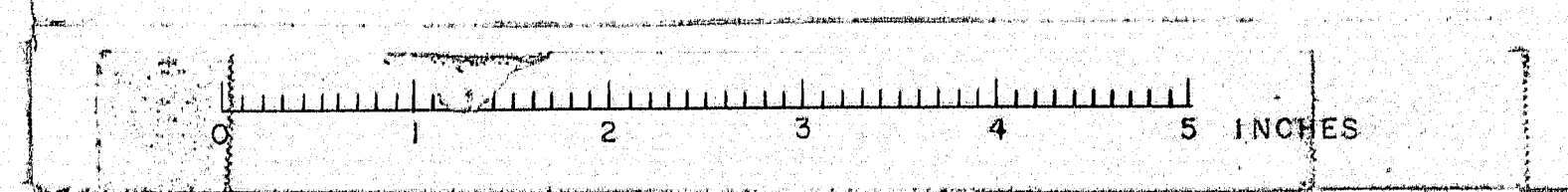
BRIDGE NO.
SURVEY -
PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

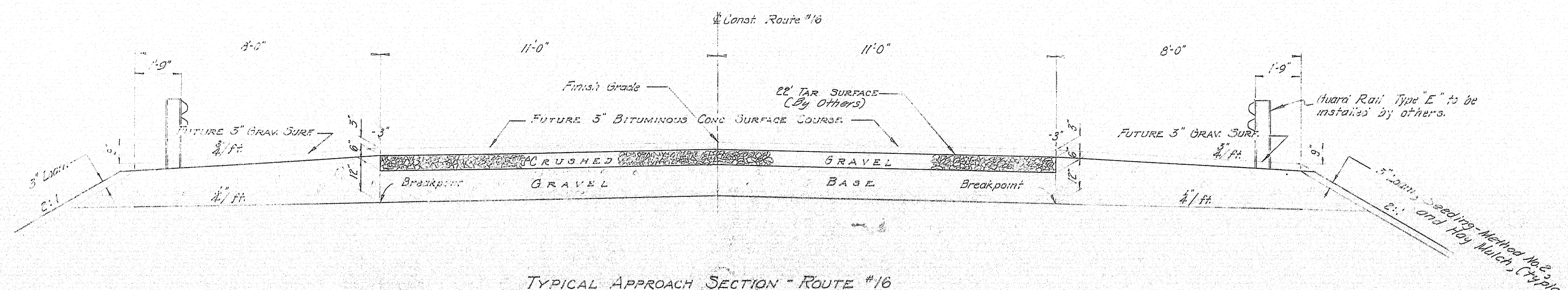
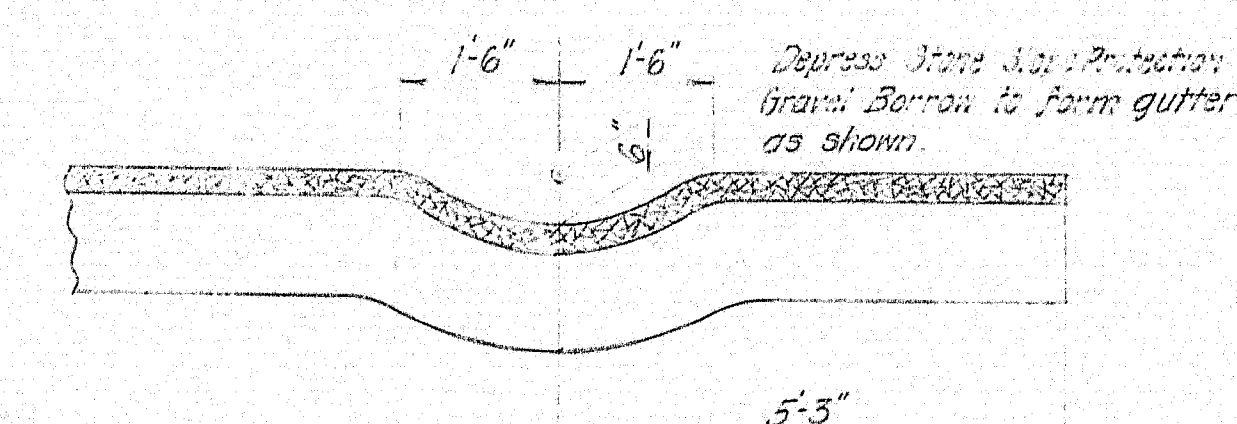
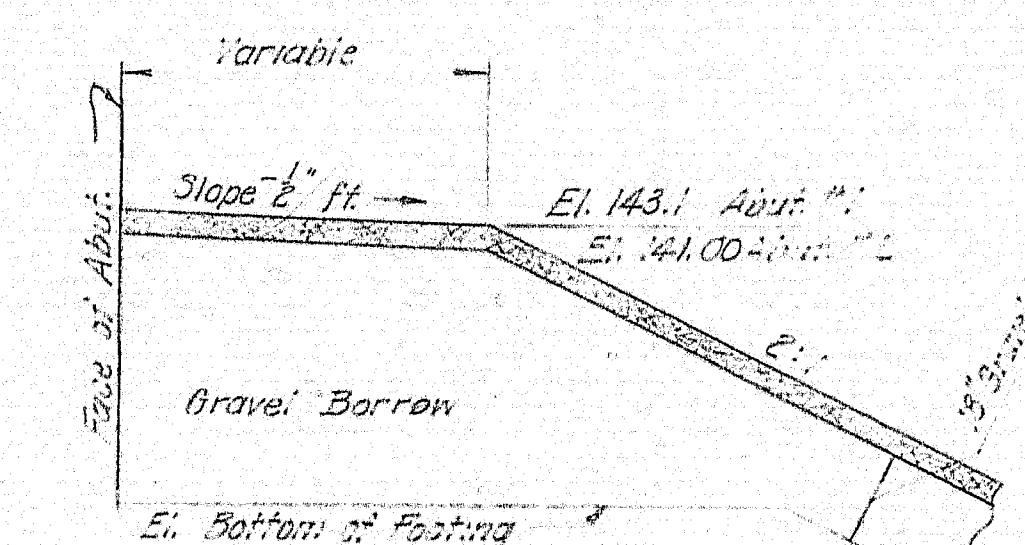
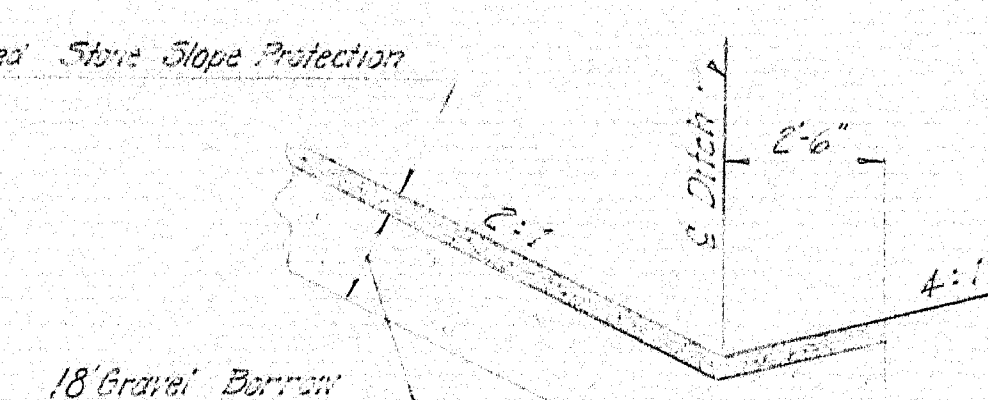
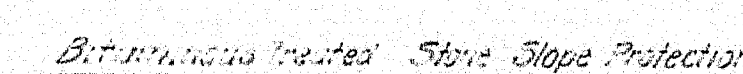
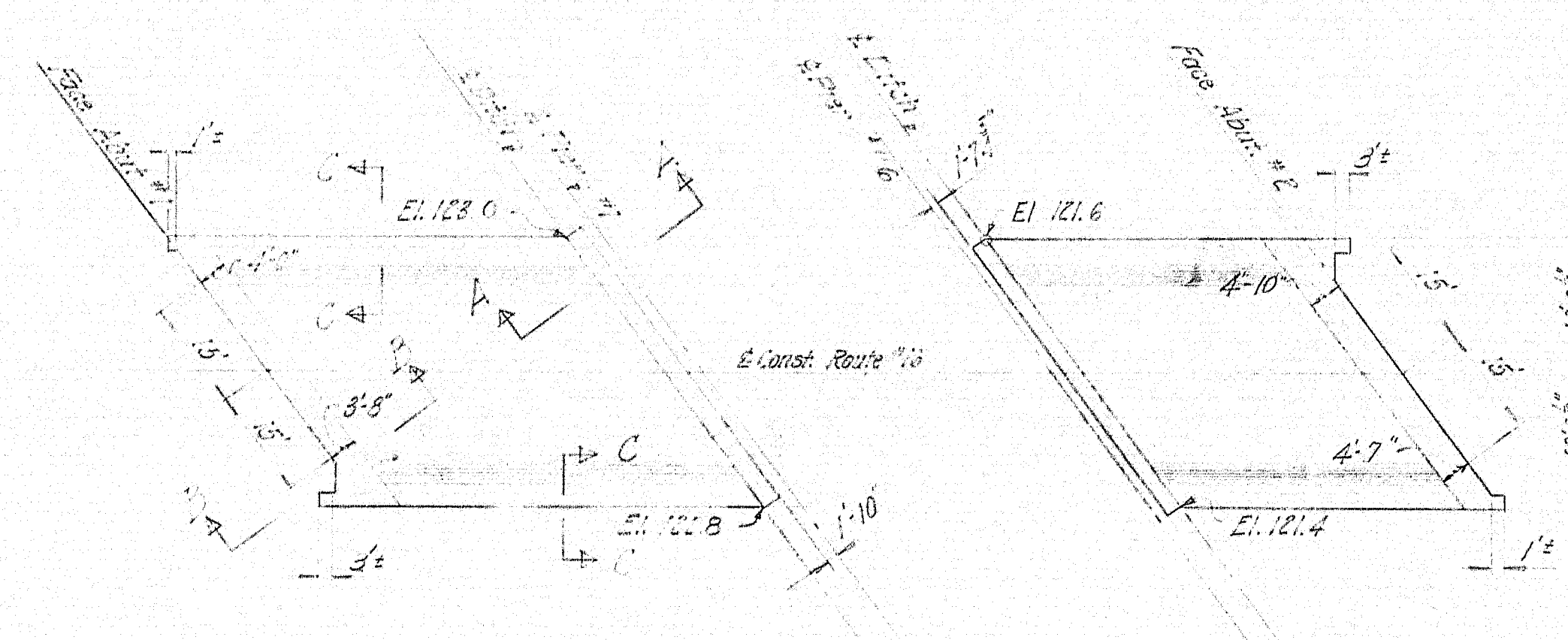
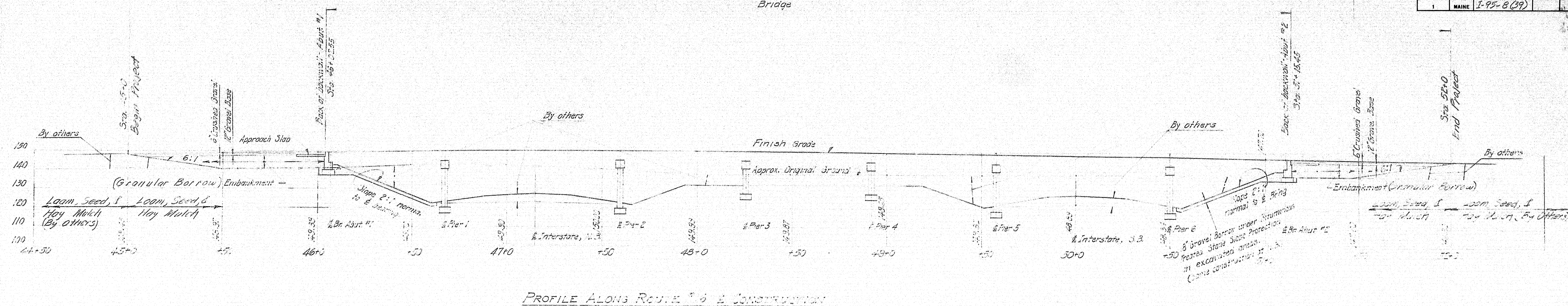
ROUTE 16
OVER
INTERSTATE 95
IN THE CITY OF
OLD TOWN
PENOBSCOT COUNTY
BORING DETAILS

SHEET 4 OF 17 AUGUSTA, MAINE JAN. 1964

92-87



B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEET
1	MAINE	1-95-8(39)		



1. The telephone service will be installed in the main building.
2. It is the opinion of the Board that the material under the terms of the contract is of such quality that the same should be accepted in full.
3. Any necessary alterations to the contract should be made by the Board.
4. Sections 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829,

WHITE.

5. The lines of the Bureau Radio-Electric Company and the Consolidated Telephone Company will remain in substantially their present location along existing Route 16 during construction of the bridge.

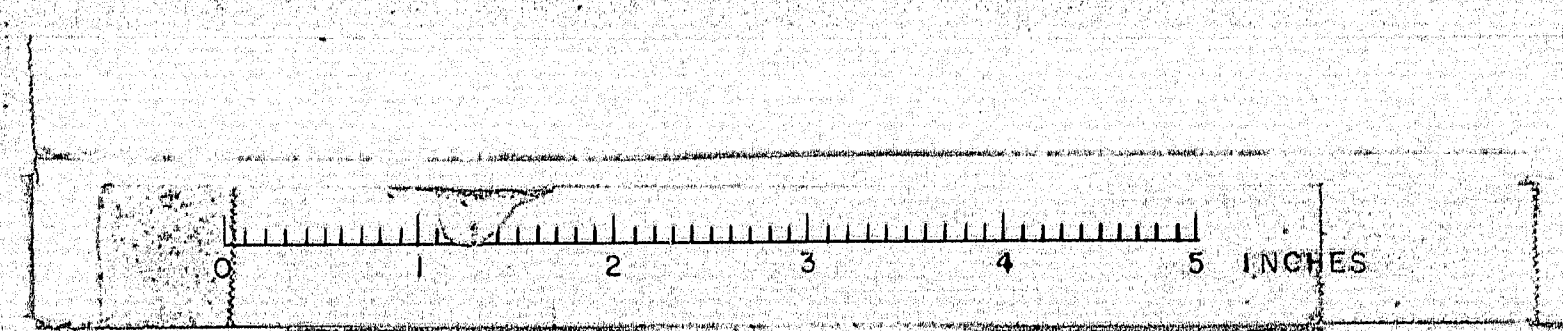
DESIGN-- W/HY	BRIDGE NO.
TRACE--	SURVEY--
CHECK-- A.L.	PLOT--

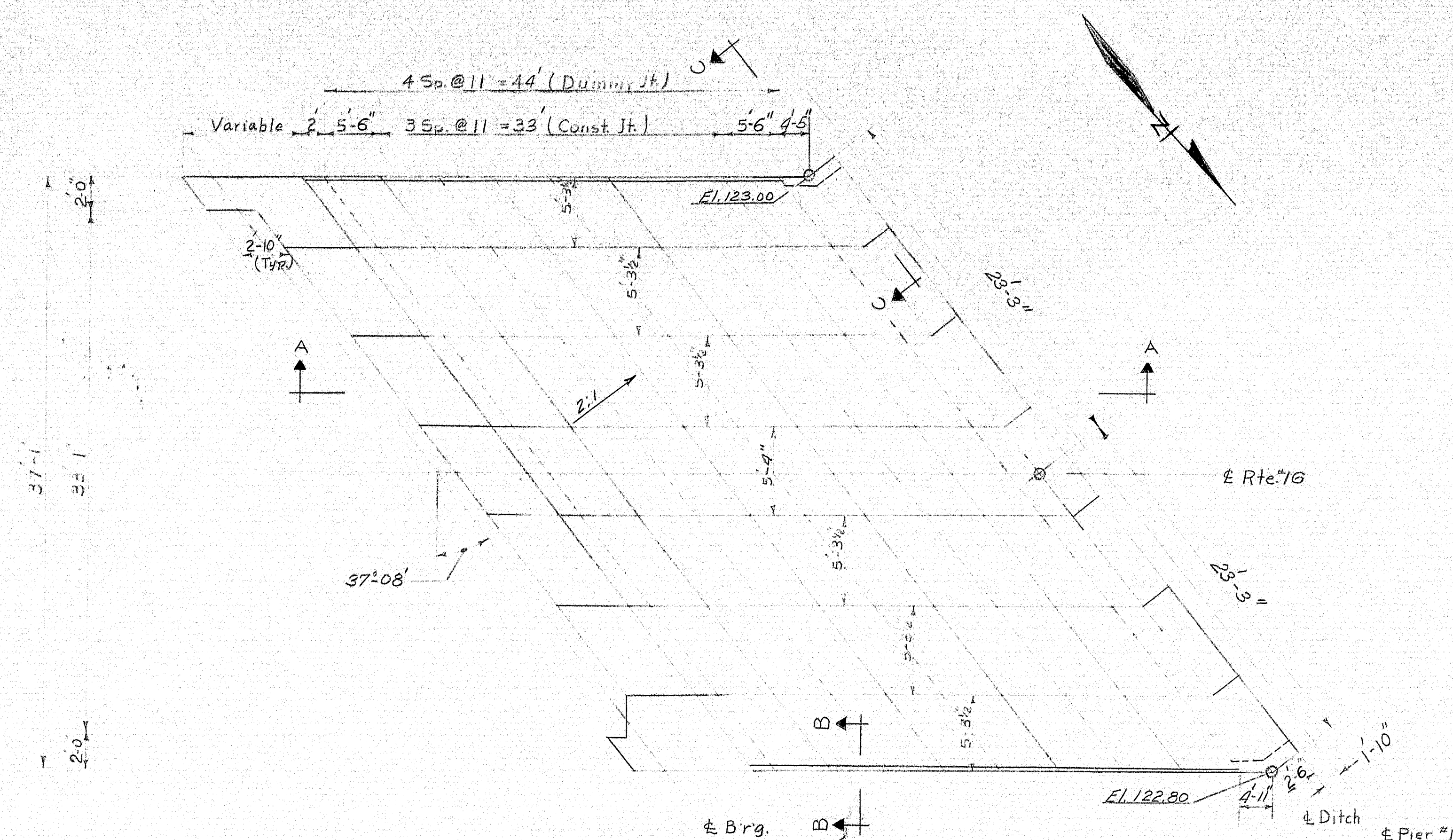
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

ROUTE 16
OVER
INTERSTATE 95
IN THE CITY OF
OLD TOWN
PENOBSCOT COUNTY

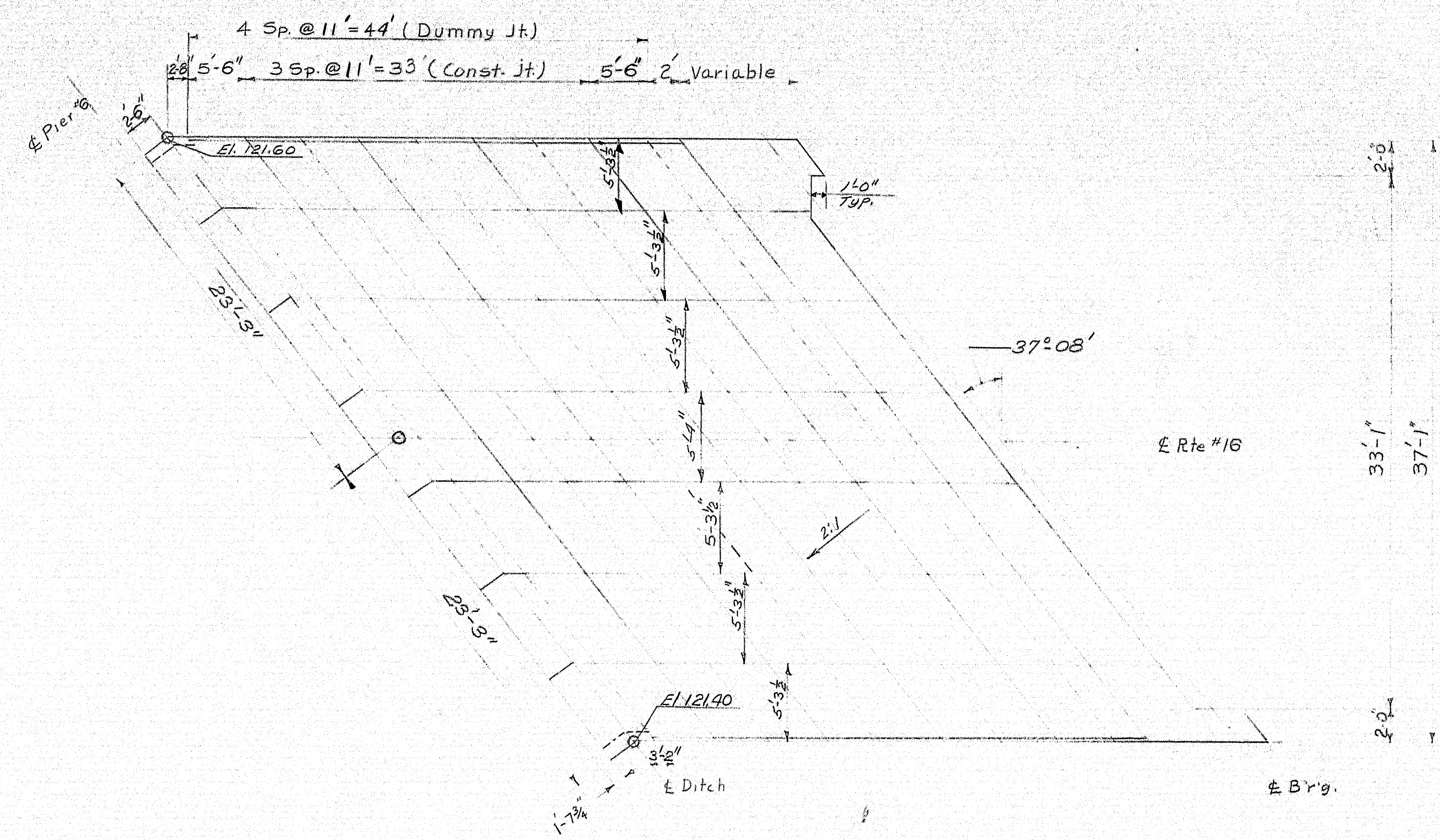
ROADWAY WORK & SLOPE PROTECTION

SHEET 5 OF 17 AUGUSTA, MAINE JAN. 1964

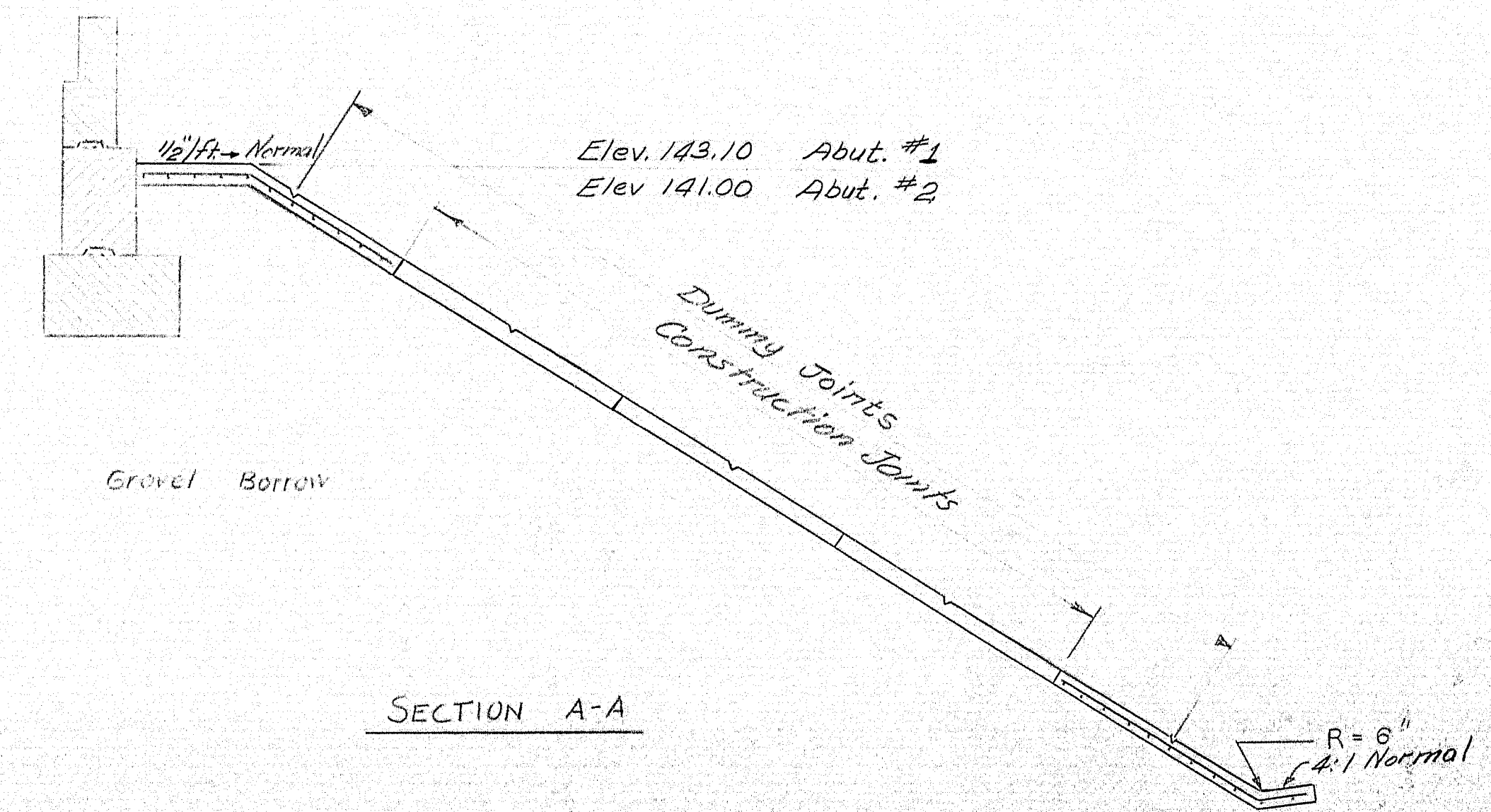




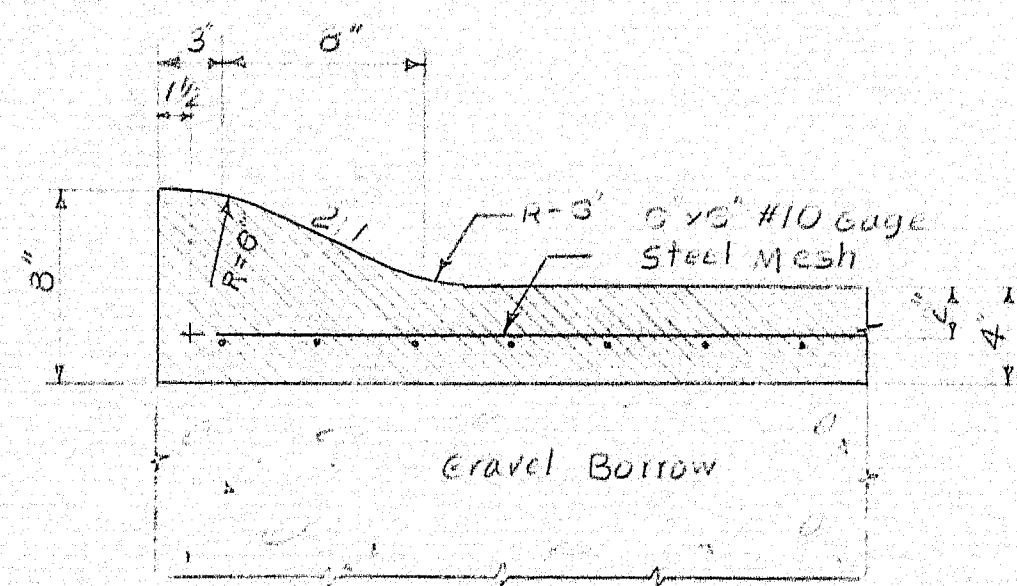
PLAN @ ABUTMENT 1



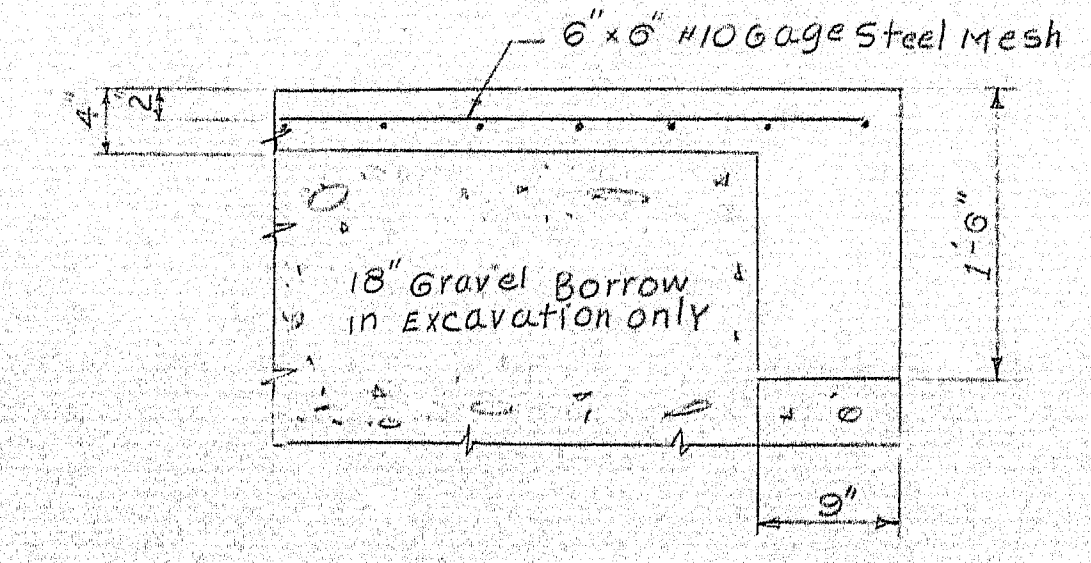
PLAN @ ABUTMENT 2



SECTION A-A



SECTION B-B



SECTION C-C

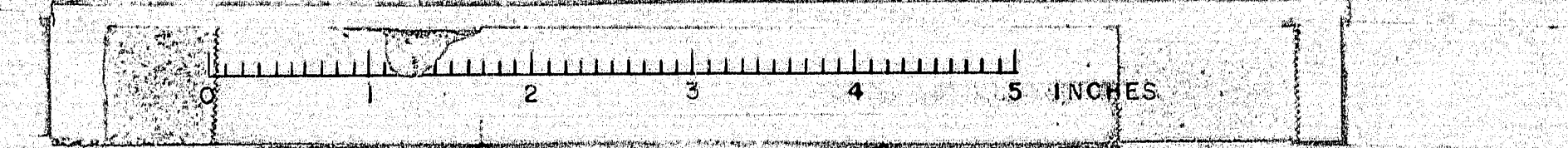
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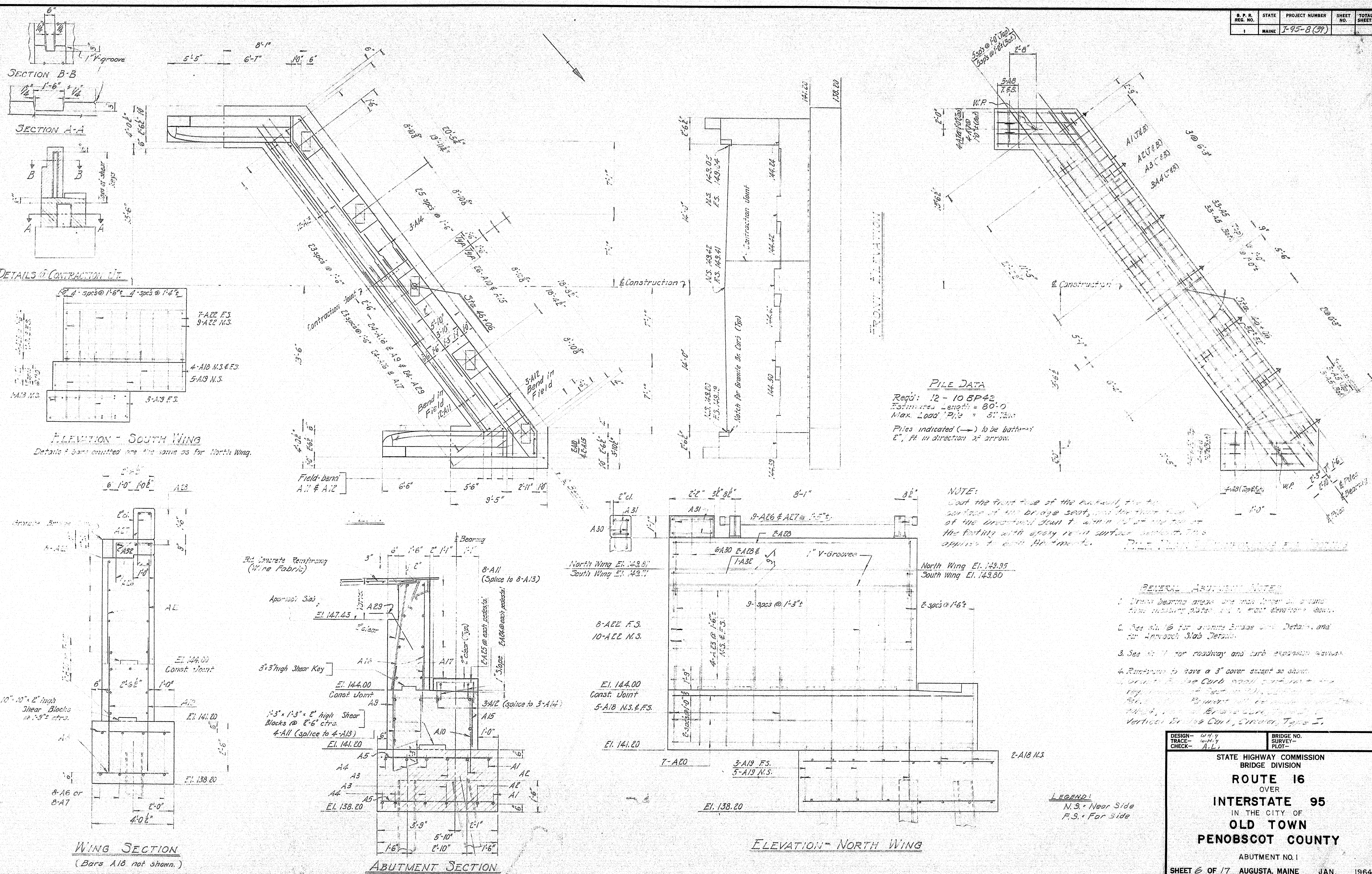
1. Break bond at construction joints with a coat of asphalt point.
2. Reinforce with #10 gage 6"x6" steel mesh, not to pass through construction joints.
3. At contractor's option, section of same strip may be cast in order Bond will be broken between adjoining sections with 1/2" Bituminous Treated Prefabricated Expansion Joint Filler edging too to a depth of 1/4".
4. Dummy joints shall be made with a sidewalk edging too to a depth of 1/4".
5. Work this sheet with sheet 5 of the construction Plans.

ESTIMATED QUANTITY CHANGES

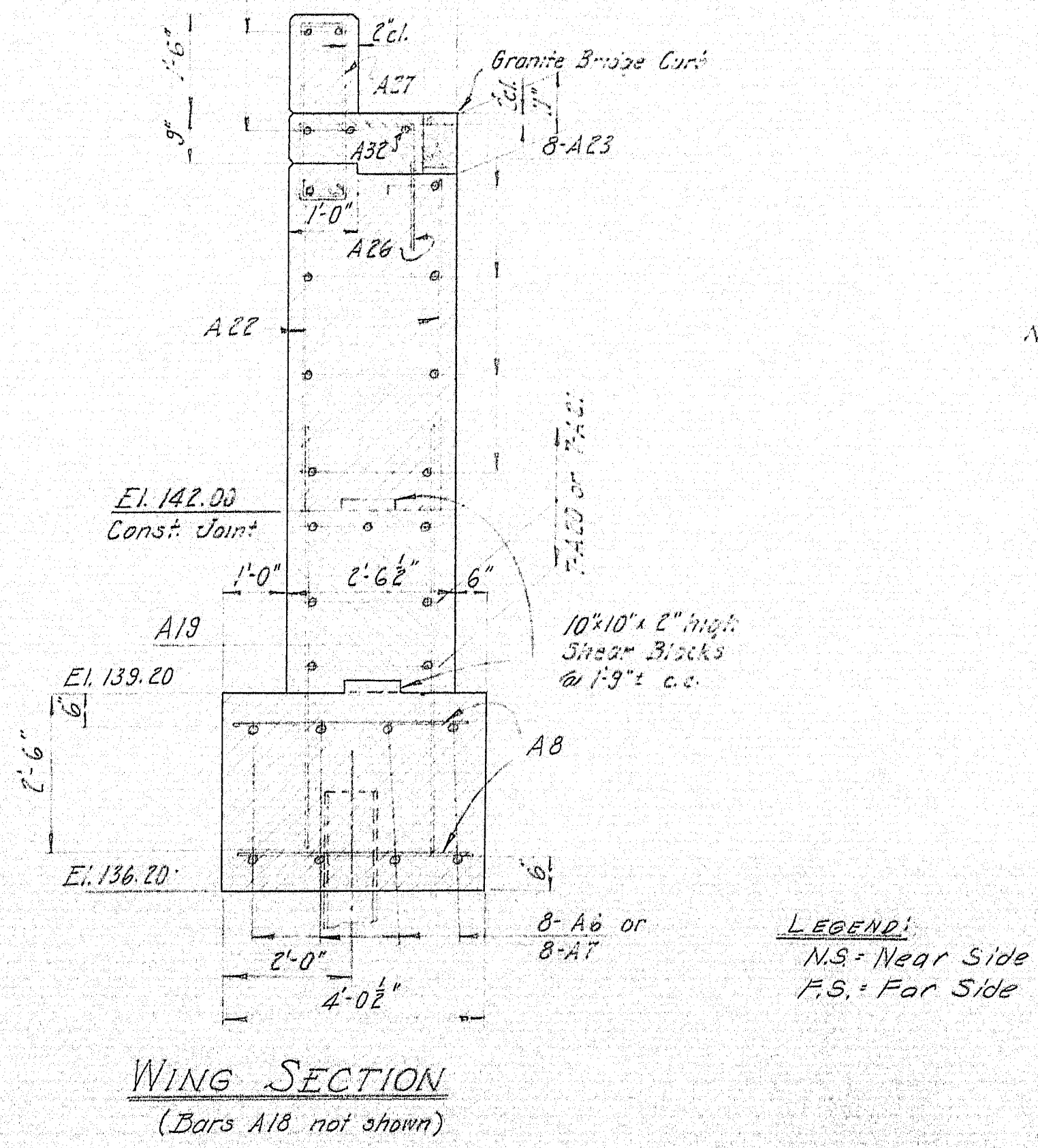
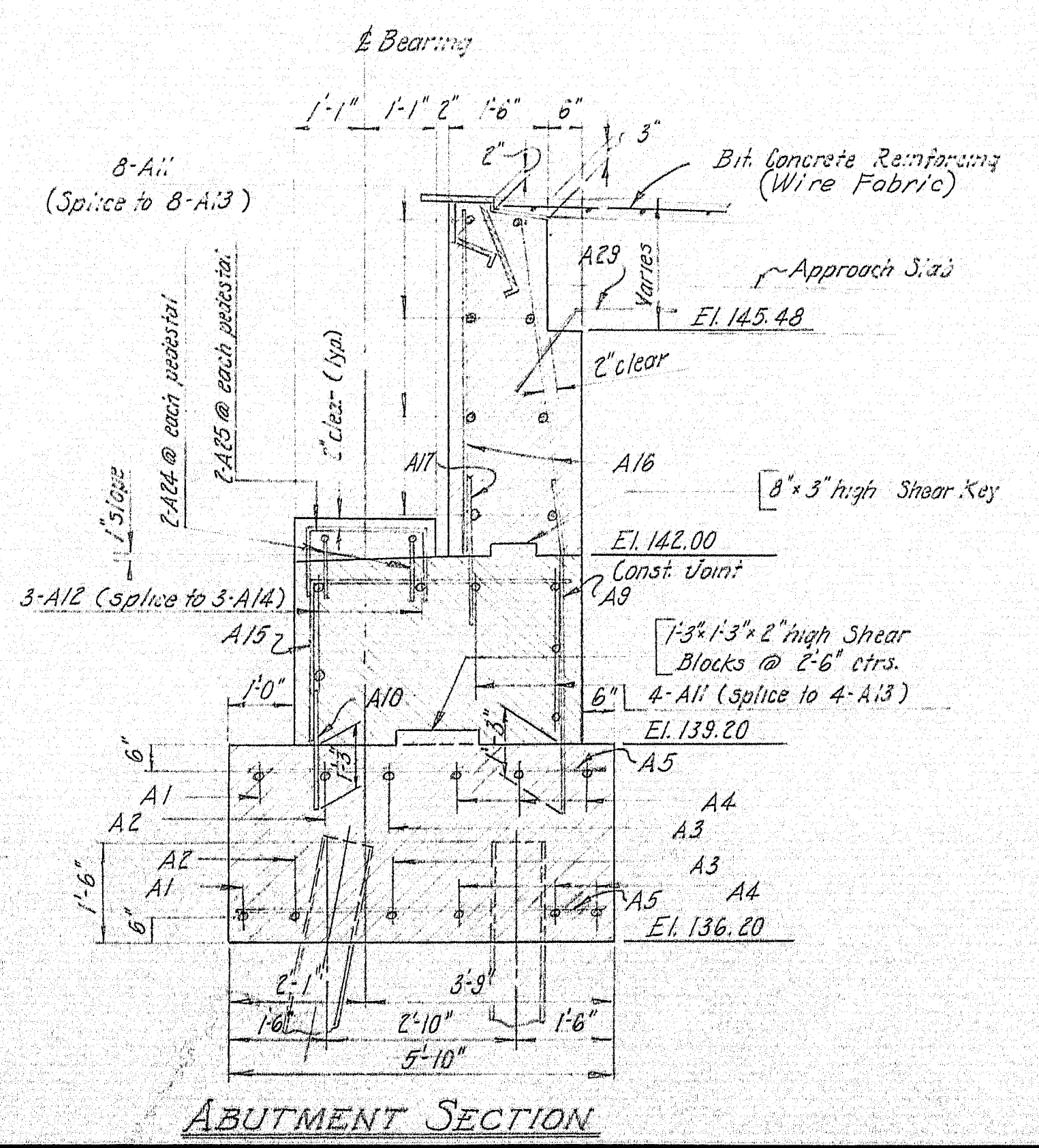
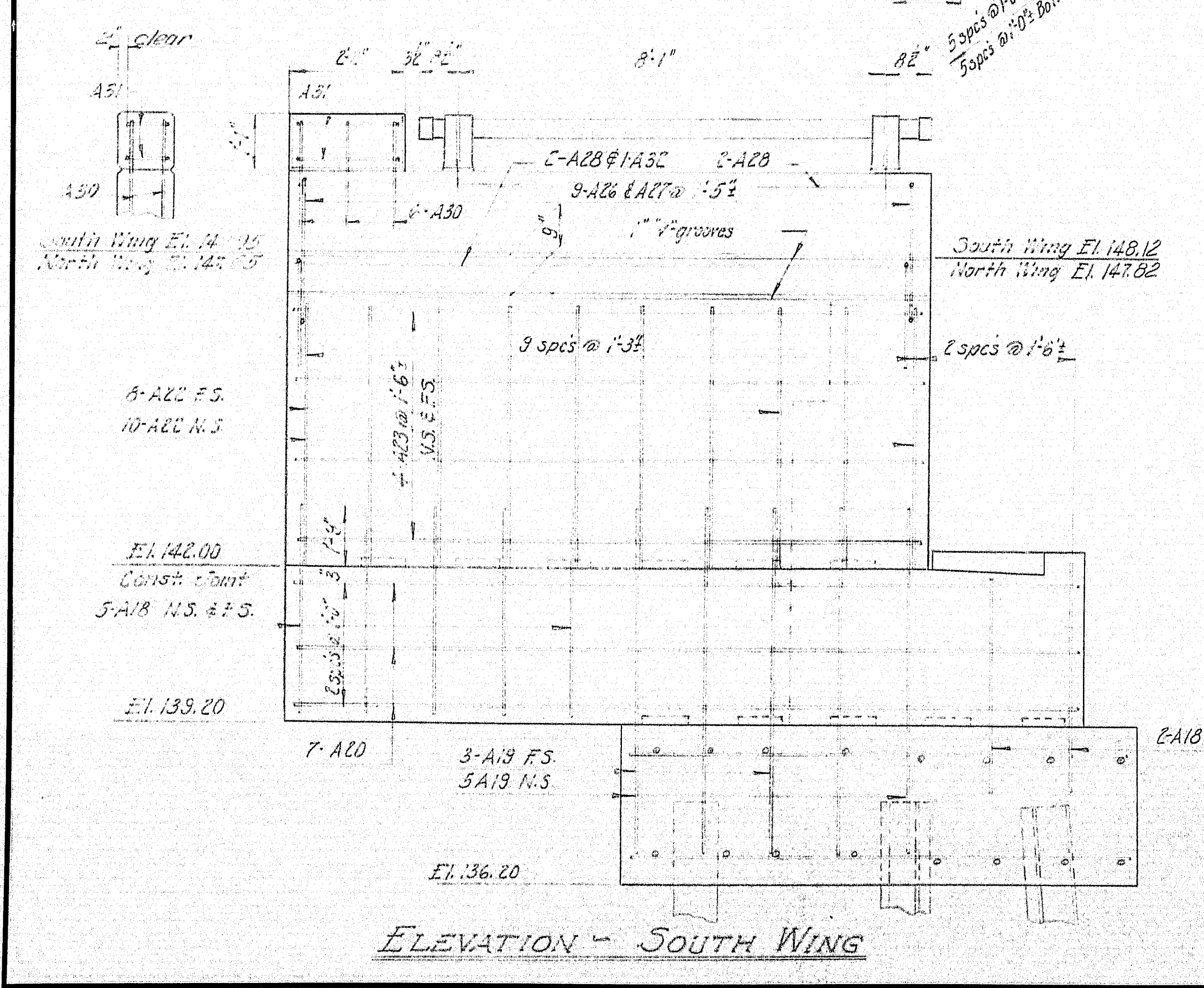
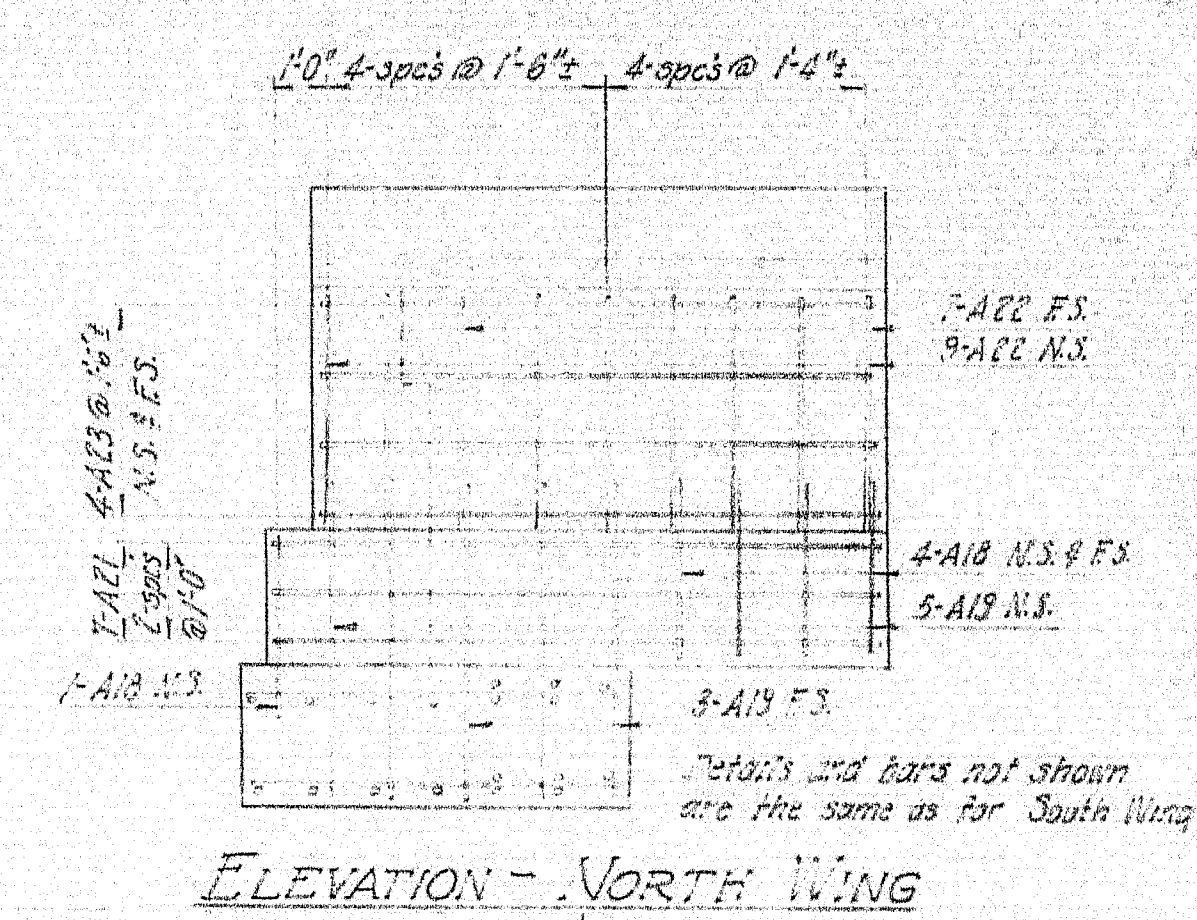
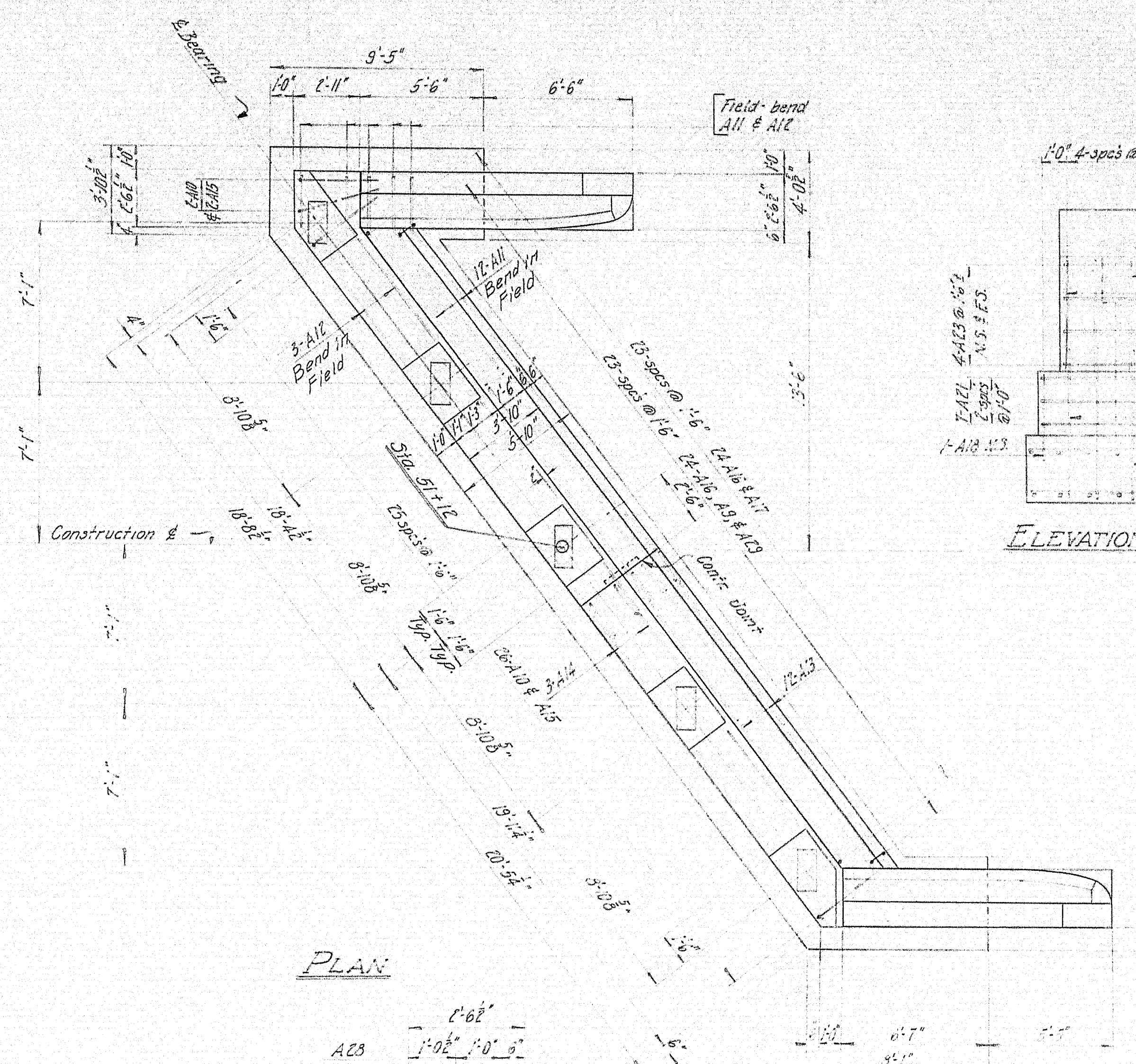
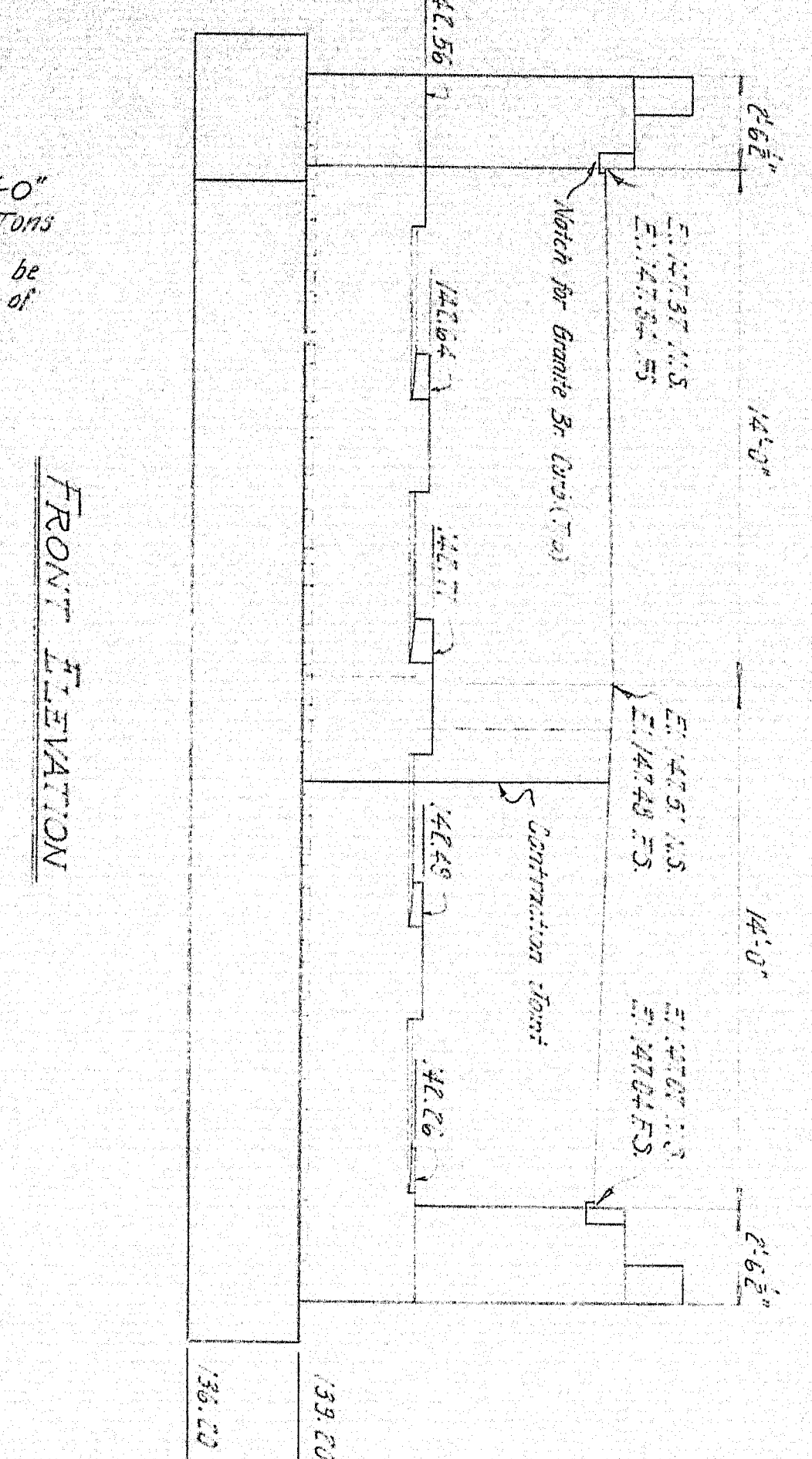
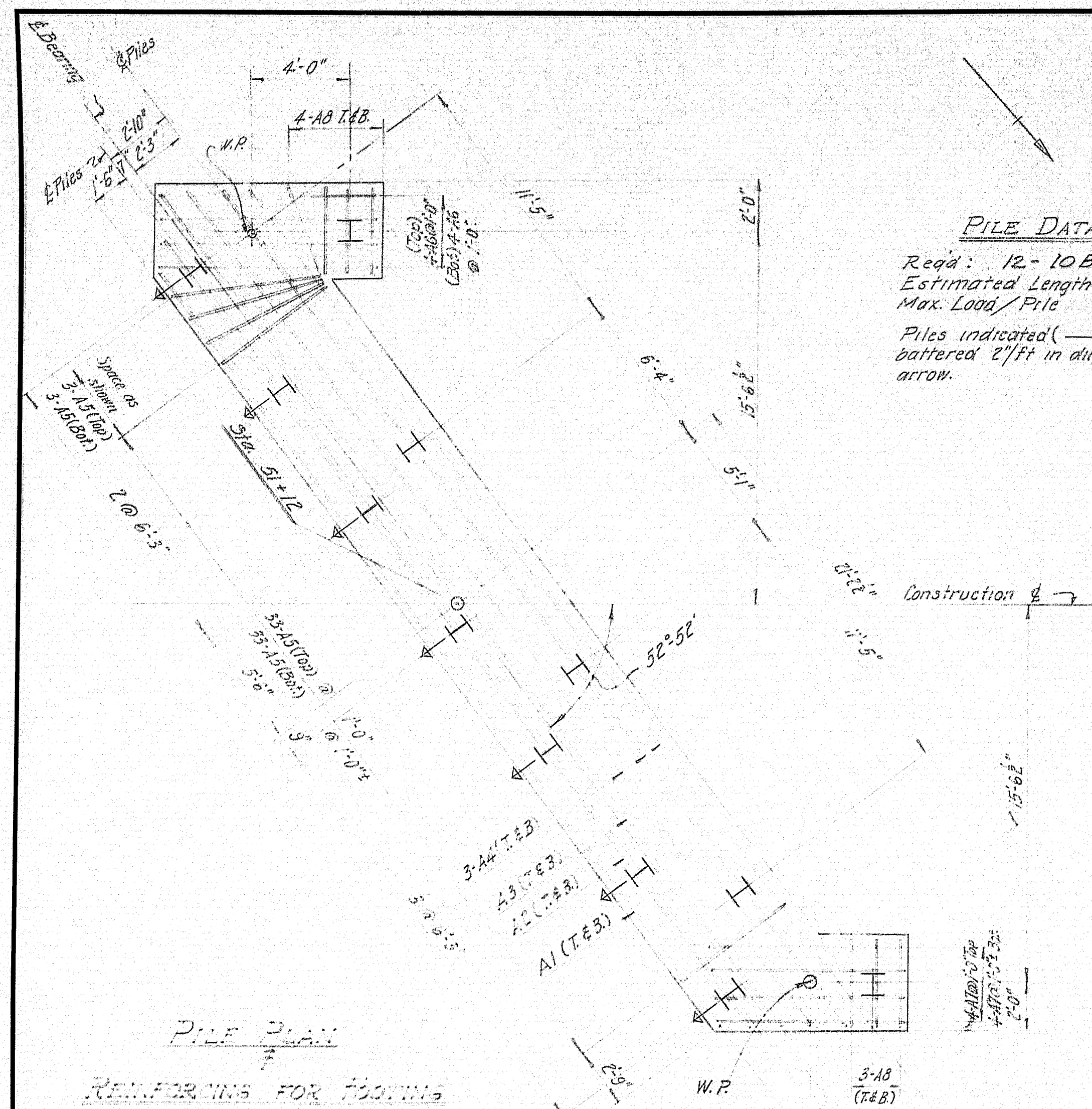
DECREASE:
Item 93-8 ~ 530 s.y. ~ Bituminous Treated Stone Slope Protection.
INCREASE:
Item 808-6 ~ 530 s.y. ~ Slope Paving.

DESIGN - R. MAJAH	BRIDGE NO.
TRACE - H.L.D.	SURVEY -
CHECK - H.L.D.	PLOT -
STATE HIGHWAY COMMISSION	
BRIDGE DIVISION	
ROUTE 16	
OVER	
INTERSTATE 95	
IN THE CITY OF	
OLD TOWN	
PENOBSCOT COUNTY	
SLOPE PAVING	
SHEET 54 OF 17 AUGUSTA, MAINE JULY 1964	





D. P. R.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
REG. NO.	MAINE	I-95-B (39)	7	17

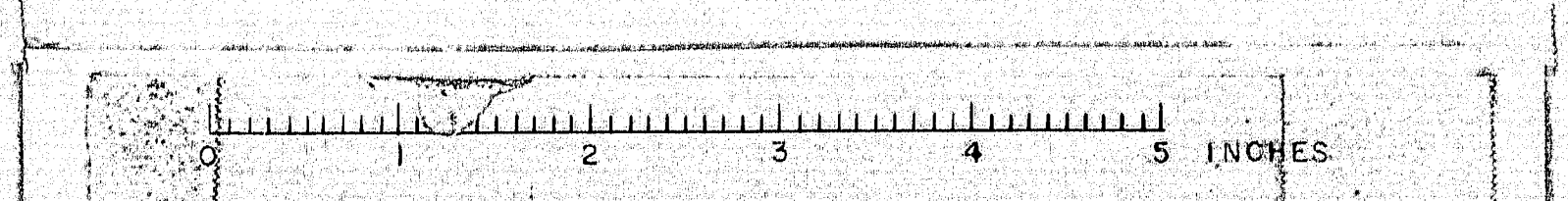


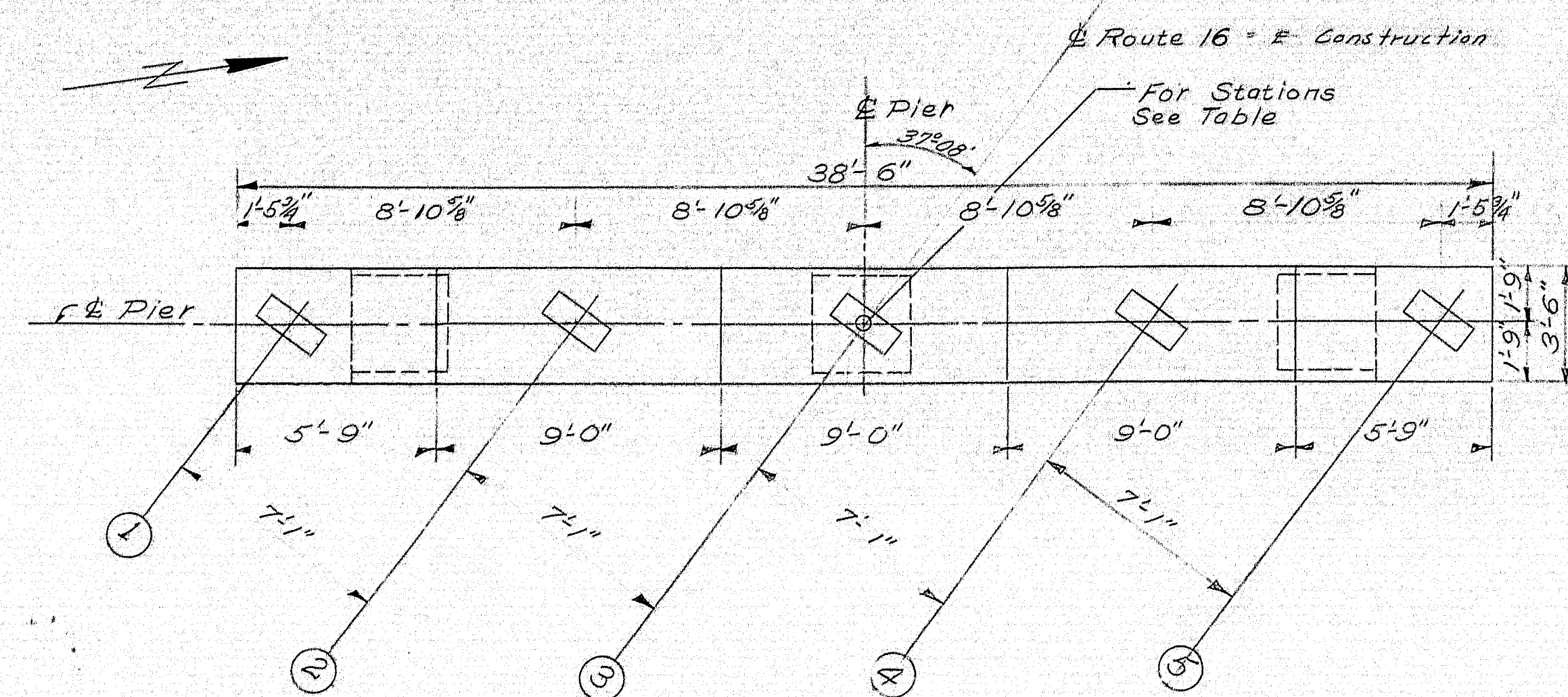
Note:
 See ch 5 for Bridge Construction Notes
 See ch 6 for Details in Construction Notes

LEGEND:
 N.S. - Near Side
 F.S. - Far Side

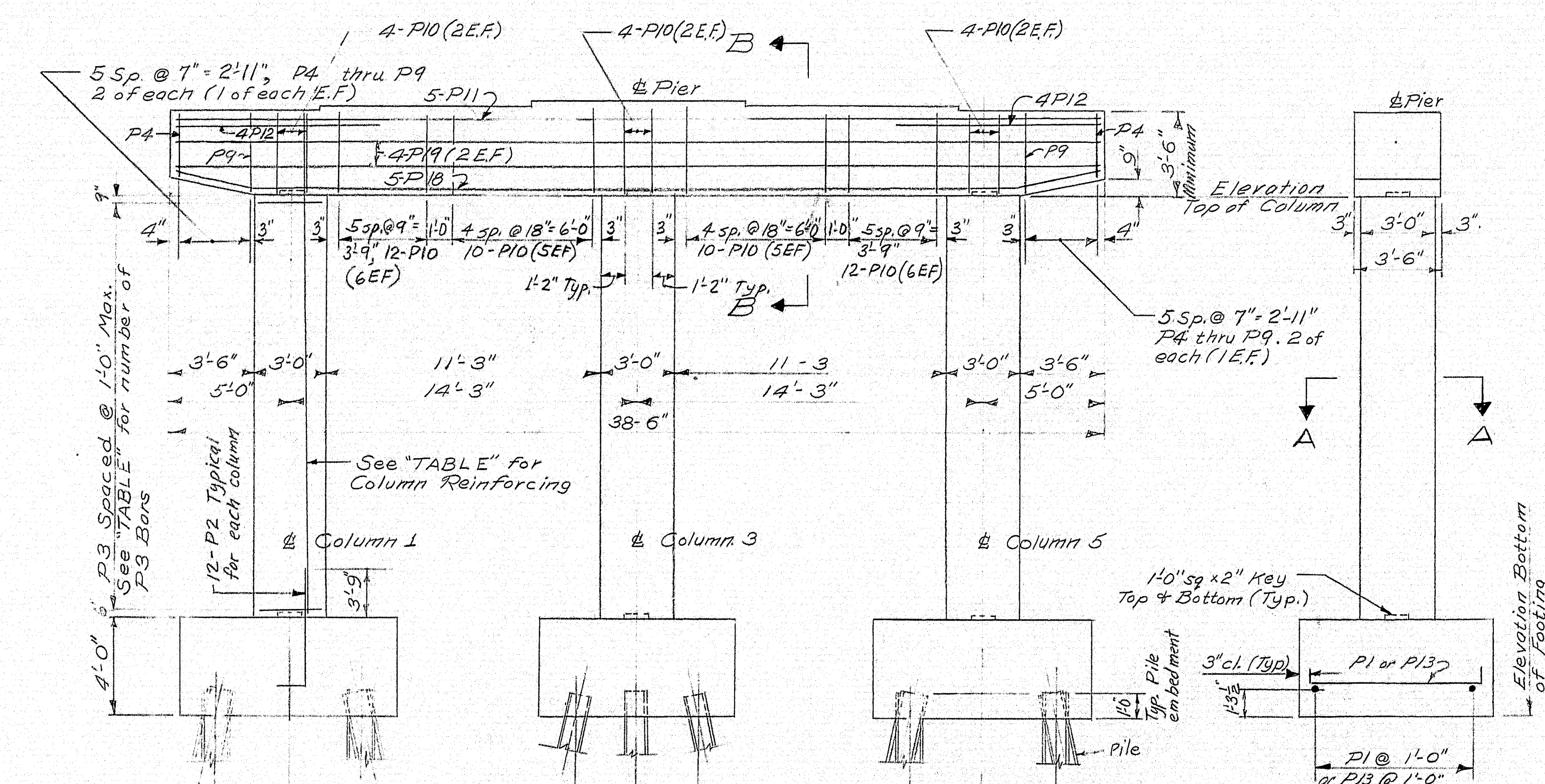
DESIGN - W.H.V.	BRIDGE NO.
TRACE - W.H.V.	SURVEY -
CHECK - A.L.	PLOT -

STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
ROUTE 16
 OVER
INTERSTATE 95
 IN THE CITY OF
OLD TOWN
PENOBSCOT COUNTY
 ABUTMENT NO. 2
 SHEET 7 OF 17 AUGUSTA, MAINE JAN. 1964



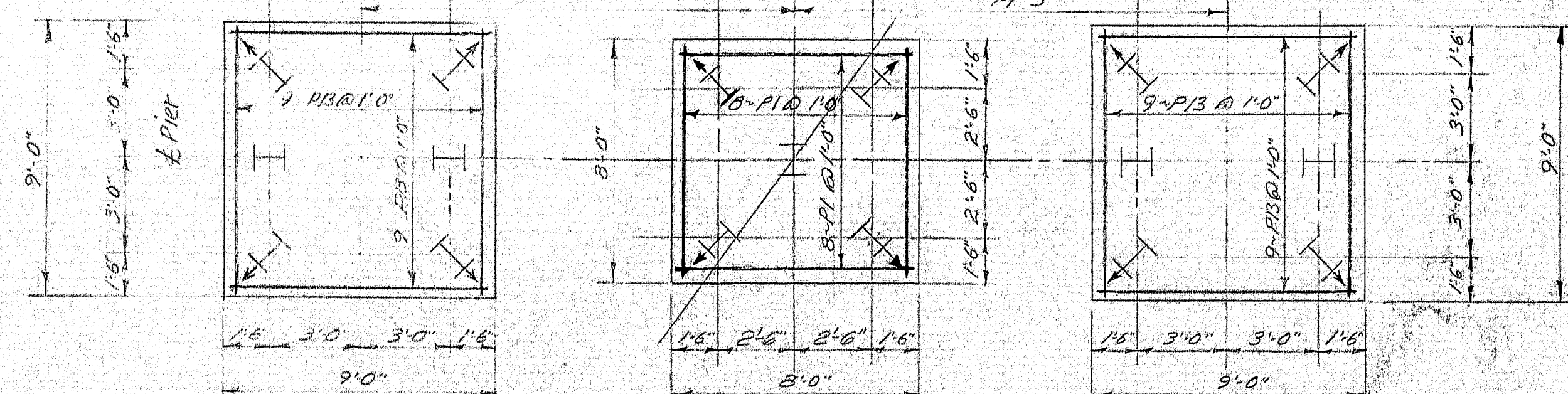


PLAN-PIER CAP



SIDE ELEVATION

END ELEVATION



PILE & FOOTING PLAN

Location Line	Pier #1	Pier #2	Pier #5	Pier #6
①	144.06	144.51	143.85	142.80
②	144.23	144.66	143.96	142.88
③	144.40	144.81	144.07	142.97
④	144.28	144.66	143.88	142.75
⑤	144.15	144.52	143.68	142.54

TABLE					
PIER	COLUMN	Elevation Bottom of Footing	Column Reinf.	Station	Elevation Top of Column
#1	1	117.50	12-P14 19-P3	46+68	140.5
	3	117.50	do		
	5	117.50	do		
#2	1	117.50	12-P15 20-P3	47+60	141.01
	3	117.50	do		
	5	117.50	do		
#5	1	116.50	12-P16 20-P3	49+58	140.18
	3	116.50	do		
	5	116.50	do		
#6	1	116.50	12-P17 19-P3	50+50	139.0
	3	116.50	do		
	5	116.50	do		

Pier	No. of Piles	Estimated Length
#1	17	45 ft.
#2	17	40 ft.
#3	14	55 ft.
#4	14	55 ft.
#5	17	45 ft.
#6	17	45 ft.
Total	96	

The image contains two hand-drawn architectural drawings of column sections, labeled SECTION A-A and SECTION B-B.

SECTION A-A
 Typical for all columns
 This drawing shows a rectangular column cross-section. The overall width is 3'0" and the overall height is 3'0". The width is divided into segments of 2 1/8", 9 1/8", 1 1/8", 9 1/8", and 2 1/8". The height is divided into segments of 2 1/8", 9 1/8", 1 1/8", 9 1/8", and 2 1/8". The drawing shows a grid of reinforcement bars (P1 through P12) and a central core. A note indicates "3" clear" for the top bars. A note "P1 thru P12" points to the reinforcement bars. A note "P3" points to a specific bar. A note "2" Min Clear all around" is written at the bottom left.

SECTION B-B
 This drawing shows a rectangular column cross-section. The overall width is 3'6" and the overall height is 3'6". The width is divided into segments of 3 1/8", 8 3/8", 8 3/8", 8 3/8", and 3 1/8". The height is divided into segments of 3 1/8", 1'0", 1'0", 1'0", and 3 1/8". The drawing shows a grid of reinforcement bars (P1 through P12) and a central core. A note indicates "3" minimum" for the top bars. A note "P11 Top Bars" points to the top reinforcement bars. A note "P4 thru P10" points to the central reinforcement bars. A note "P12" points to a specific bar. A note "P13" points to a specific bar. A note "2" Min Clear all around" is written at the bottom left.

Pile Specs: (Piles 4" x 40' @)

1. All piles shall be 30" x 42" steel H-piles.
2. Maximum pile load = 37 tons.
3. Piles are to be driven to design or practical refusal in compact and bearing.
4. Piles marked "us" to be battered 2 inches per foot in the direction of the arrow.

GENERAL NOTES:

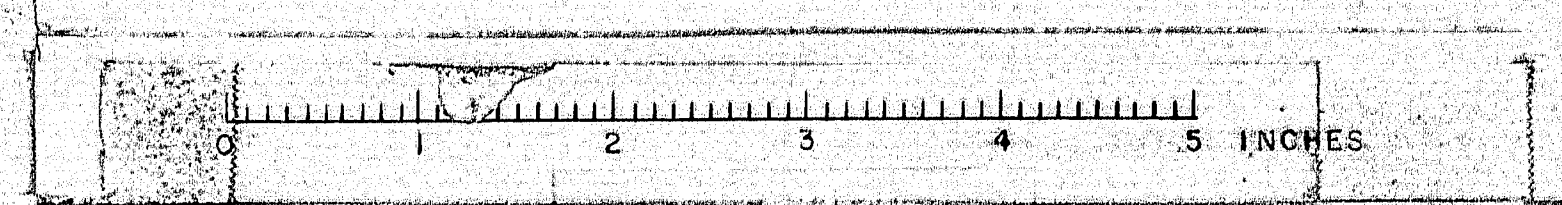
1. Position reinforcing steel in pier cap, under bearings, to clear swaged anchor bolts.
2. Chamfer all exposed edges of concrete $\frac{1}{4}$ inch.
3. Dress the shaded bearing areas on the bridge seats $1\frac{1}{2}$ inch larger all around the masonry plates, to exact elevations shown in "BRIDGE SEAT ELEVATIONS" table.
4. E.F. each face of pier.

DESIGN- *CDH*
TRACE- *WM*
CHECK- *T. H. K.*

BRIDGE NO.
SURVEY-
PLOT-

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
ROUTE 16
OVER
INTERSTATE 95
IN THE CITY OF
OLD TOWN
PENOBSCOT COUNTY
PIERS - 1, 2, 5 & 6

SHEET *8* OF *17* AUGUSTA, MAINE JAN. 1964

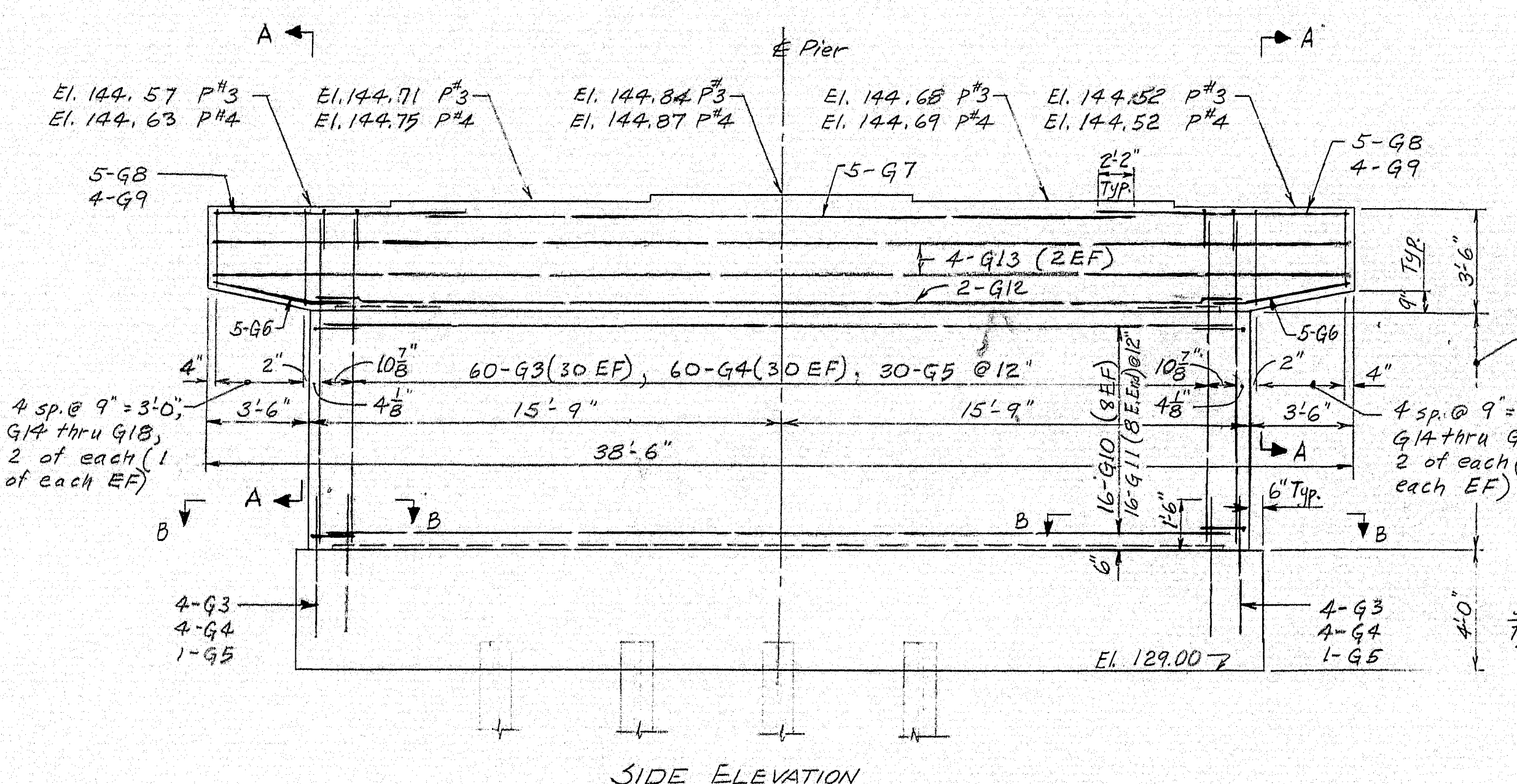
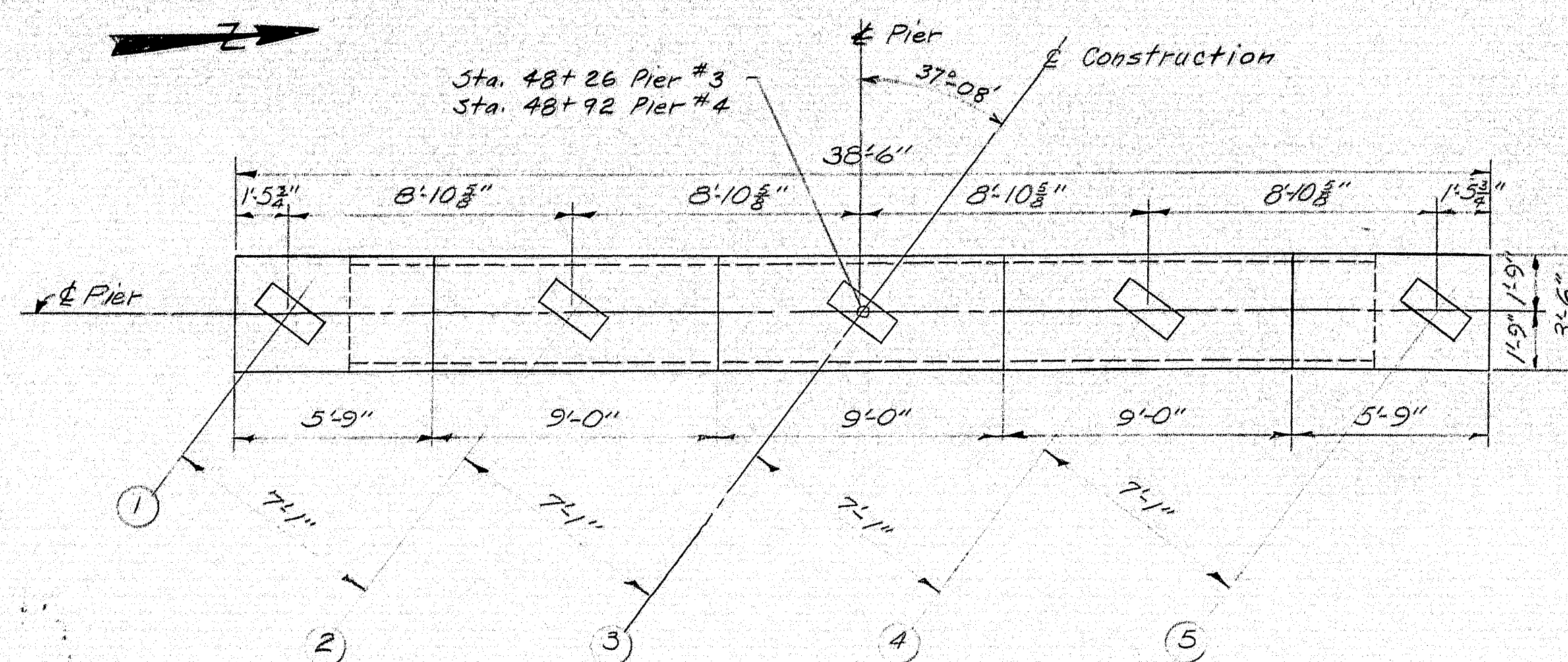


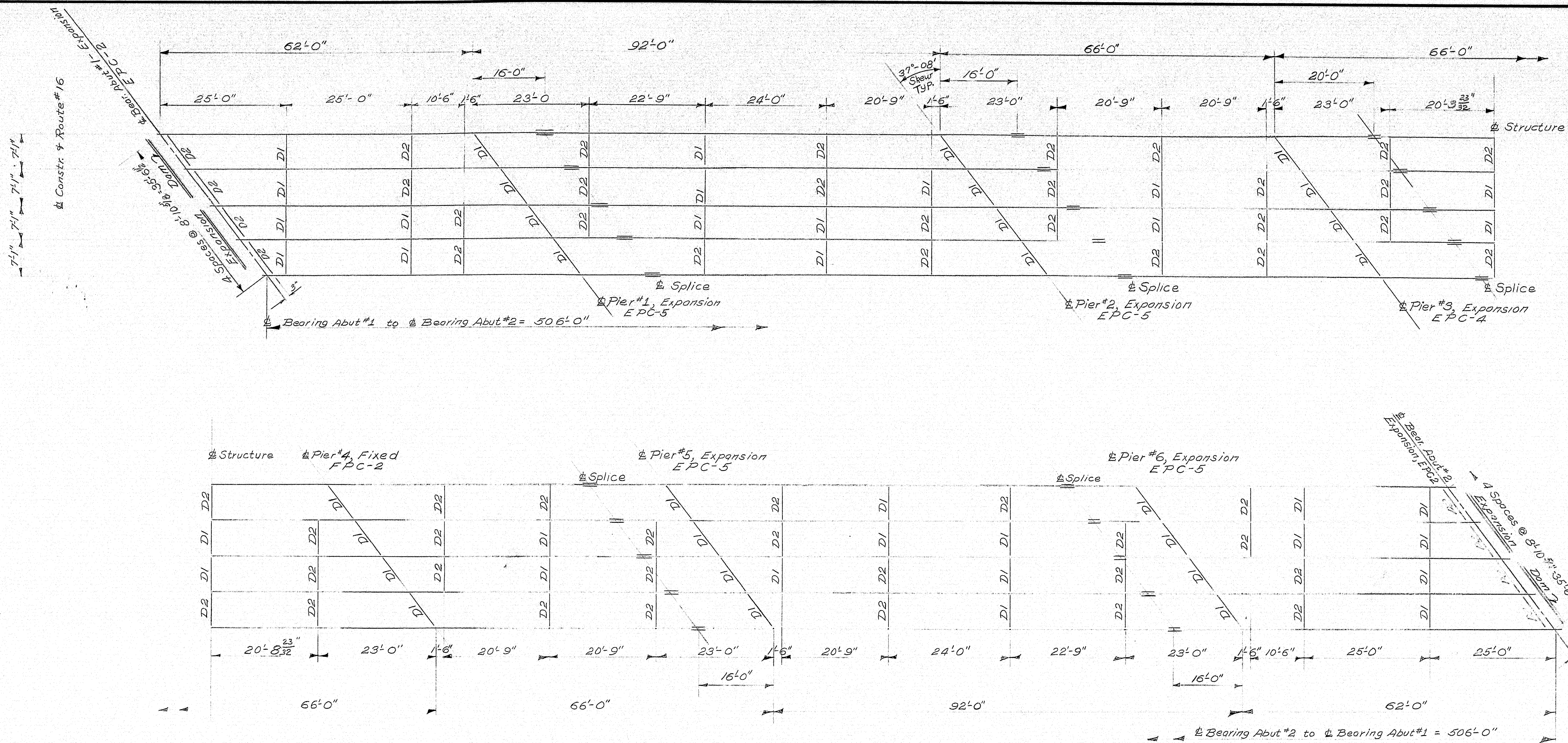
ESTIMATE OF QUANTITIES

STRUCTURAL EARTH EXCAVATION, ABUTMENTS AND RETAINING WALLS	385 C.Y.
STRUCTURAL EARTH EXCAVATION, PIERS	450 C.Y.
GRANULAR BORROW	1,580 C.Y.
GRAVEL BORROW	380 C.Y.
GRAVEL BASE COURSE - IN PLACE MEASUREMENT	290 C.Y.
CRUSHED GRAVEL BASE COURSE - IN PLACE MEASUREMENT	75 C.Y.
* BITUMINOUS CONCRETE SURFACE COURSE, TYPE "A"	175 TONS
PORTLAND CEMENT CONCRETE, ABUTMENTS AND RETAINING WALLS	180 C.Y.
PORTLAND CEMENT CONCRETE, PIERS	450 C.Y.
PORTLAND CEMENT CONCRETE, ROADWAY AND SIDEWALK SLABS ON STEEL BRIDGES	545 C.Y.
CURING BOX FOR CONCRETE CYLINDERS	
STRUCTURAL STEEL, FABRICATED AND DELIVERED	LUMP SUM
STRUCTURAL STEEL, ERECTION	LUMP SUM
STRUCTURAL STEEL, FIELD PAINTING	LUMP SUM
REINFORCING STEEL, DELIVERED	164,700 LBS.
REINFORCING STEEL, PLACING	164,700 LBS.
STEEL H-BEAM, PILES, 42 LBS/FT	6,255 LIN. FT.
BRIDGE RAIL	1,055 LIN. FT.
* MEMBRANE WATERPROOFING	1,575 S.Y.
EPXY RESIN SURFACE SEALANT	95 S.Y.
VERTICAL BRIDGE CURB, TYPE I	1,045 LIN. FT.
VERTICAL BRIDGE CURB, CIRCULAR, TYPE I	20 LIN. FT.
LOAM - IN PLACE MEASUREMENT	45 C.Y.
SEEDING, METHOD NO. 2	4.5 UNITS
HAY MULCH	0.5 TONS
BITUMINOUS TREATED STONE SLOPE PROTECTION	530 S.Y.

* Not a part of this contract

Estimated weight of structural steel including drains is 483,600 lbs.

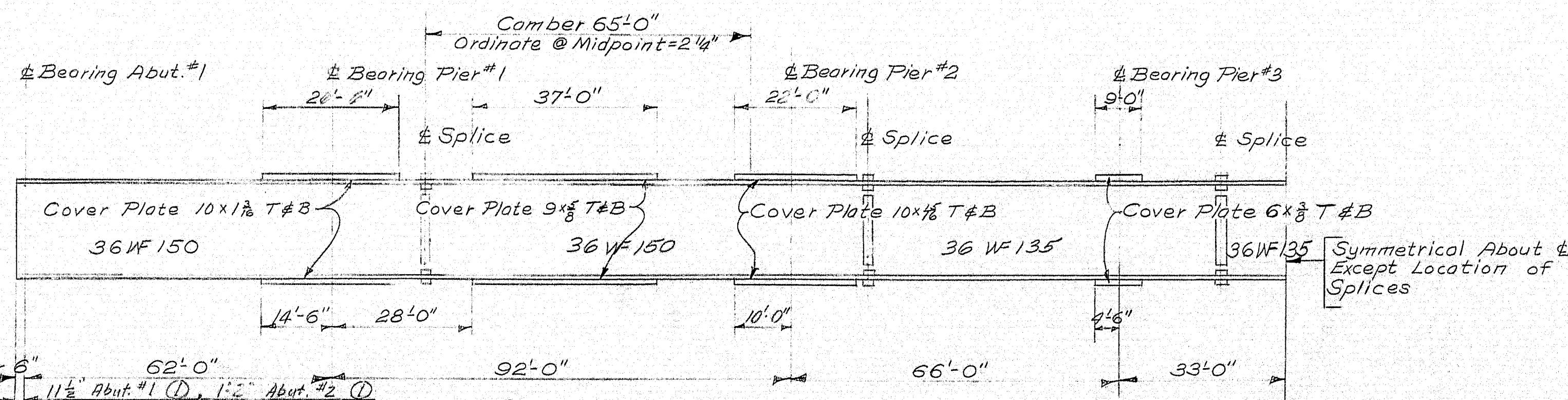




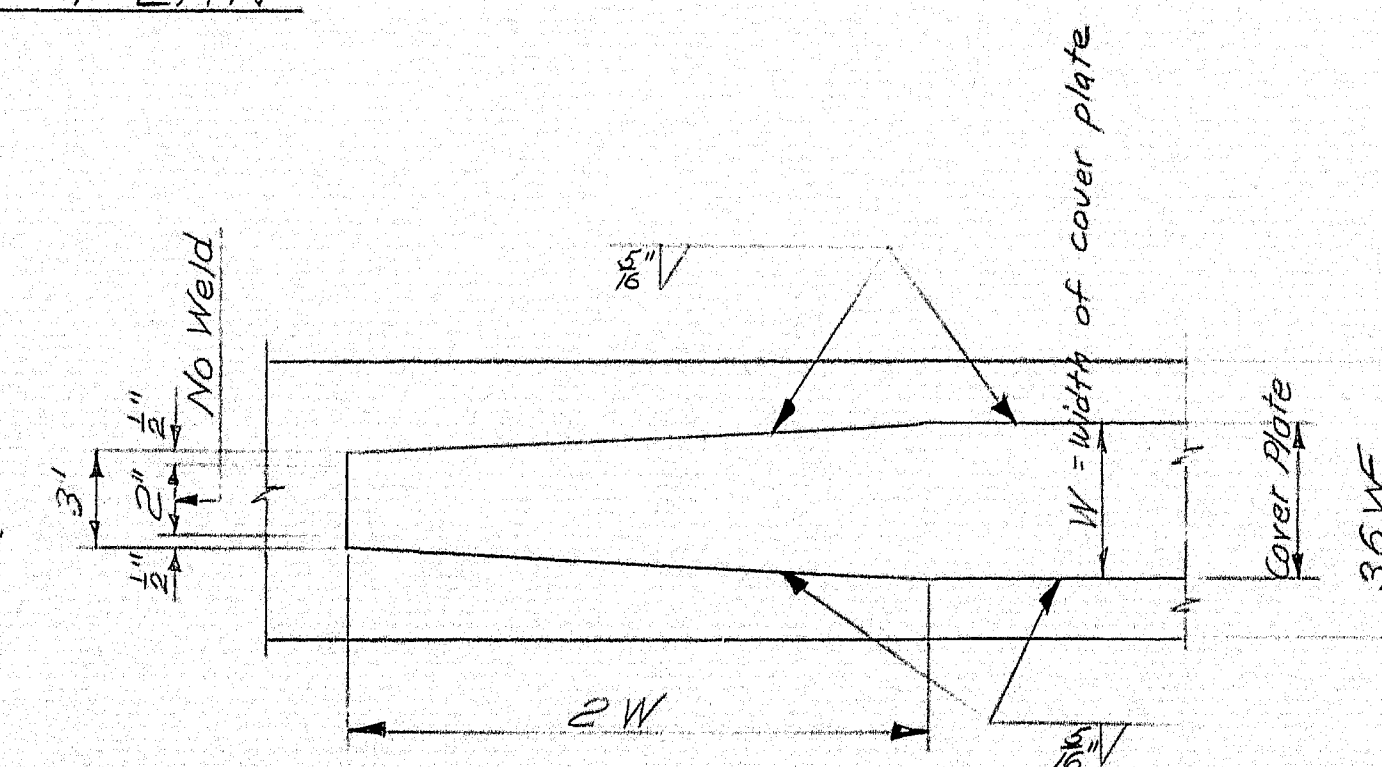
FRAMING PLAN

NOTES

1. For Structural Steel Classification see General Plan and Elevation Sheet #5.
2. Field connections will be $\frac{3}{8}$ " High Tensile Strength Bolts.
3. The Engineer shall be supplied with a diagram showing match marks of connecting parts assembled in the shop.
4. D1 are Type "A" diaphragms, D2 are Type "B" diaphragms. See Standard Details BD 104-62.
5. For Bearing Pedestals EPC-2, EPC-4, EPC-5 and EPC-2 see Standard Details BD-101-62.
6. For Splices, See Standard Details BD 104-62.
7. For Expansion Joints, See Sheet #11.

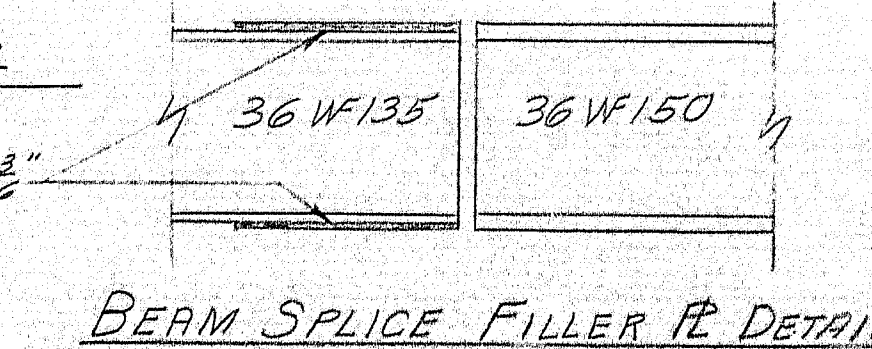


STRINGER DETAIL
All Dimensions Are Horizontal



TYPICAL STRINGER DETAIL

END OF COVER PLATE DETAIL



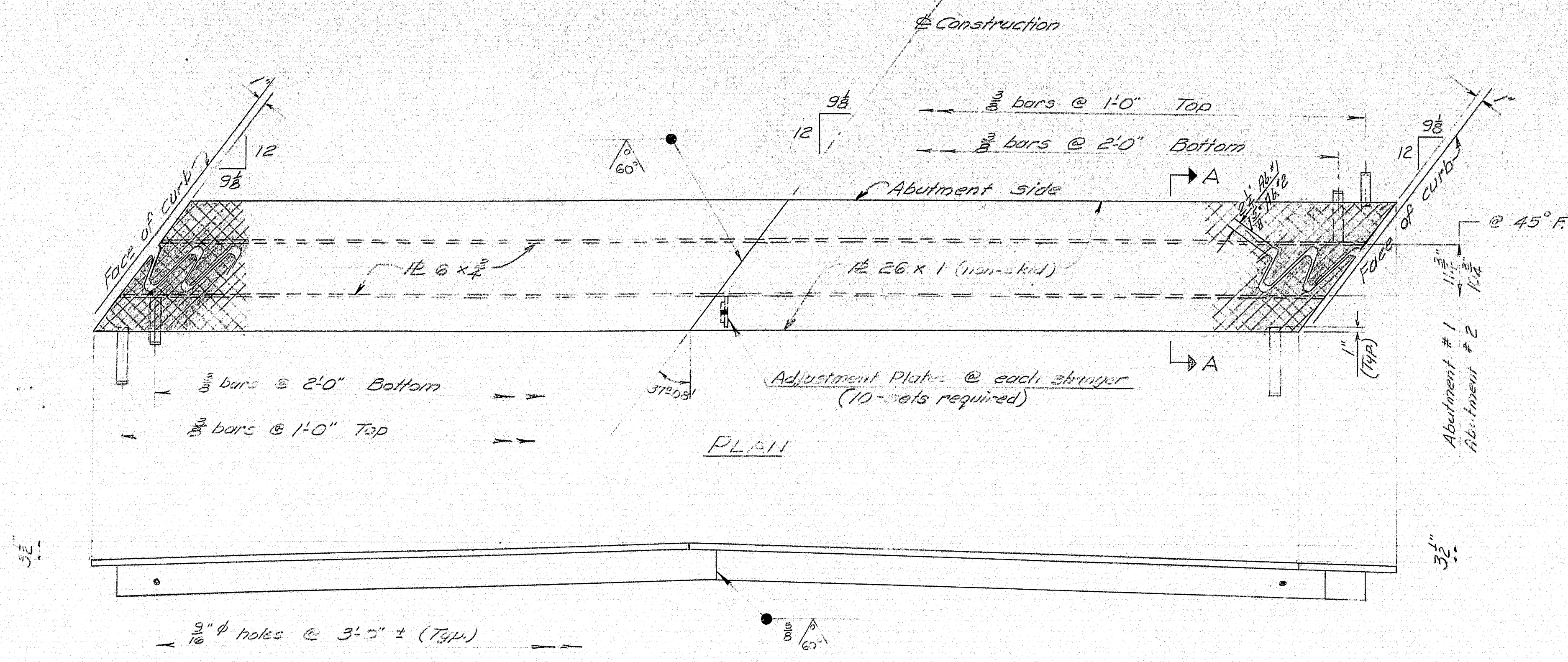
BEAM SPLICE FILLER PLATE DETAIL

FLANGE CUT DETAIL

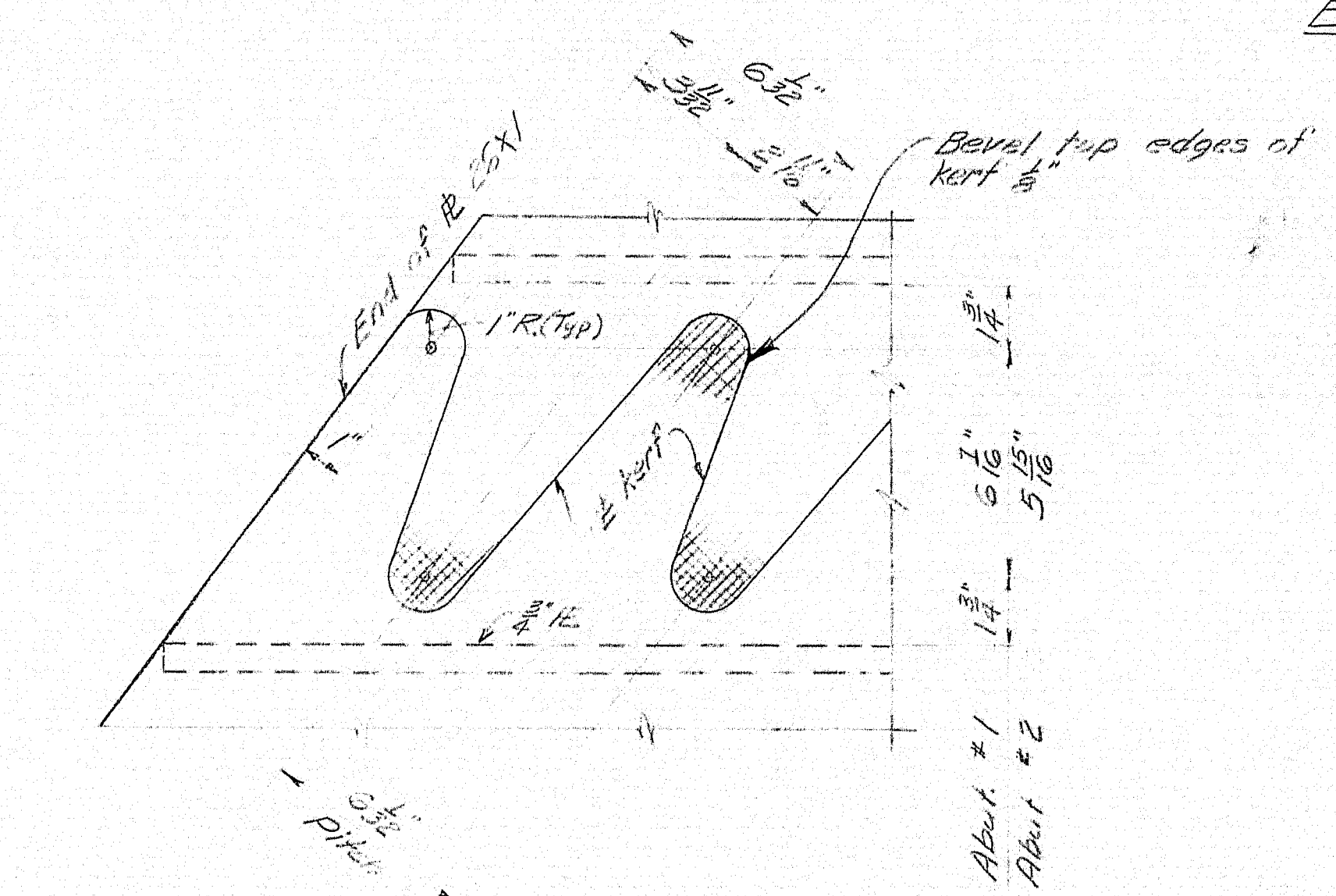
Cut top & bottom flanges of exterior stringer ① at Abut. #1 & ⑤ at Abut. #2.

DESIGN - C.D.H.	BRIDGE NO.
TRACE - DETAIL - W.J.M.	SURVEY - PLOT -
CHECK - T.H.K.	

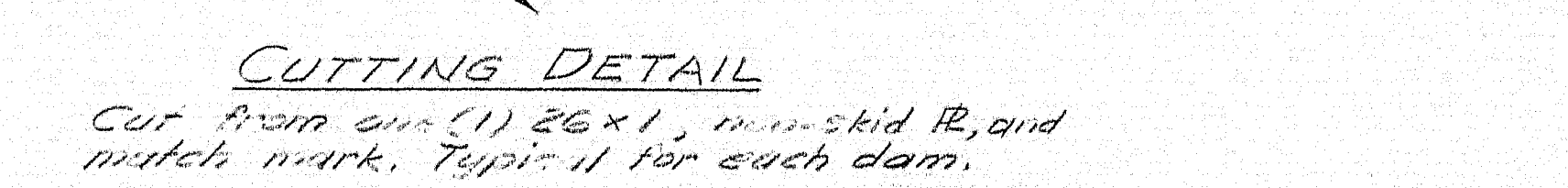
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
ROUTE 16
OVER
INTERSTATE 95
IN THE CITY OF
OLD TOWN
PENOBSCOT COUNTY
STRUCTURAL STEEL - FRAMING PLAN
SHEET 10 OF 17 AUGUSTA, MAINE JAN. 1964



PLAN

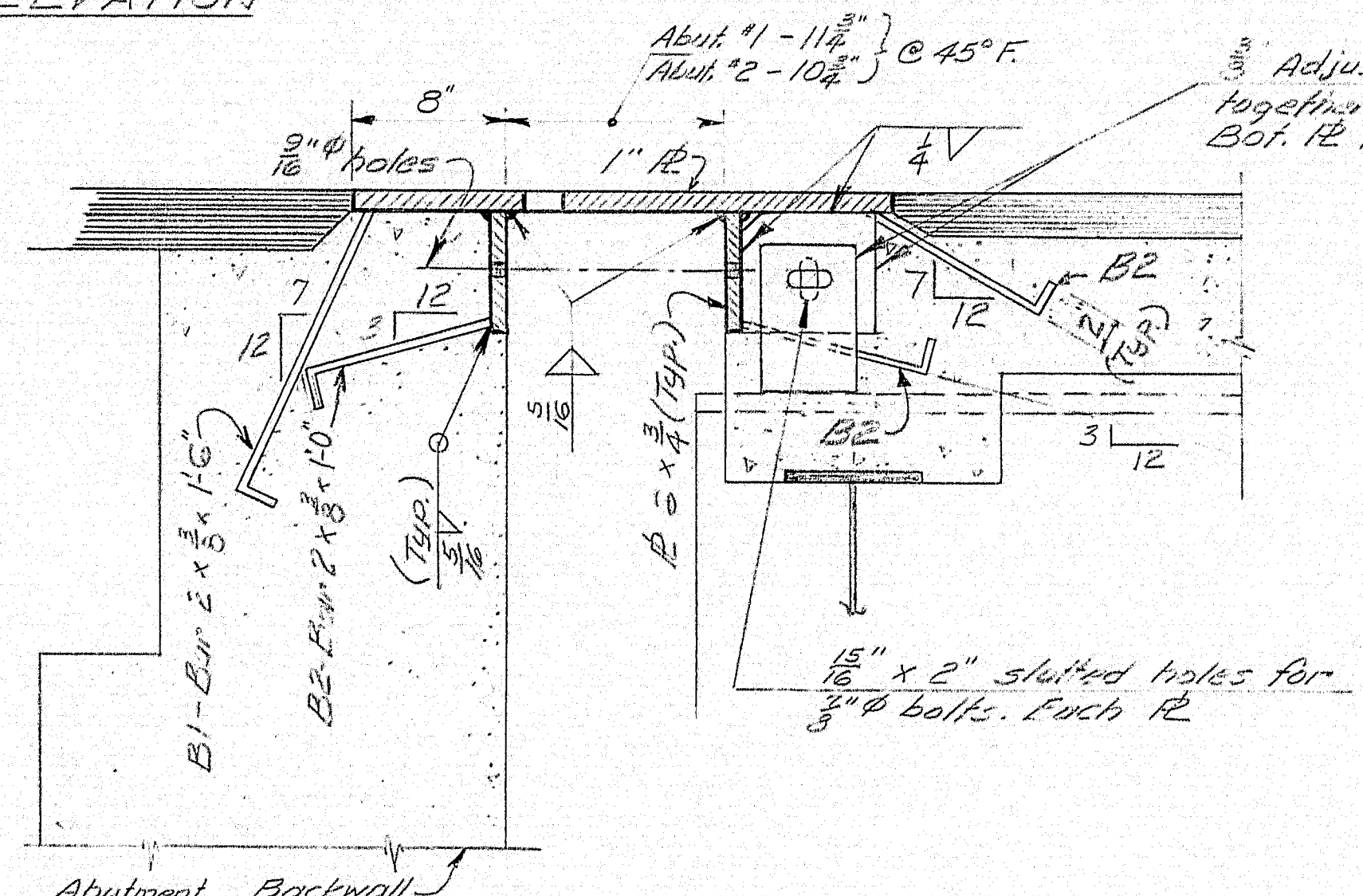


ELEVATION

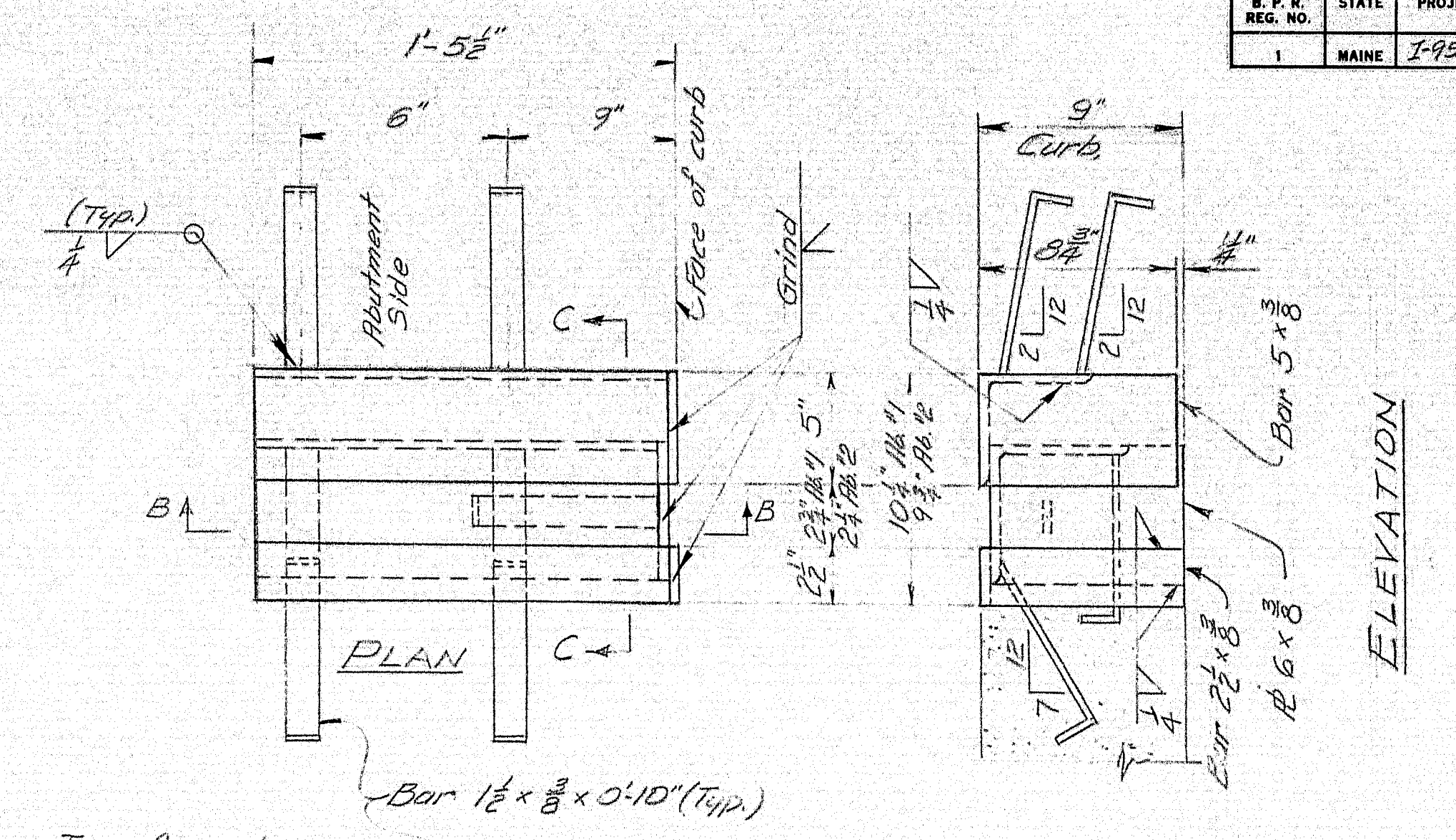


CUTTING DETAIL

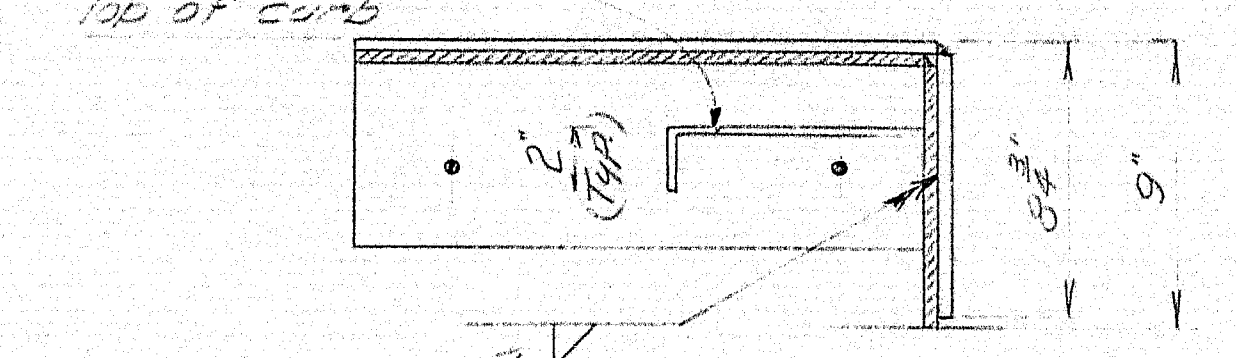
Cur from one (1) 26 x 1, non-skid R, and match mark. Typic. 11 for each dam.



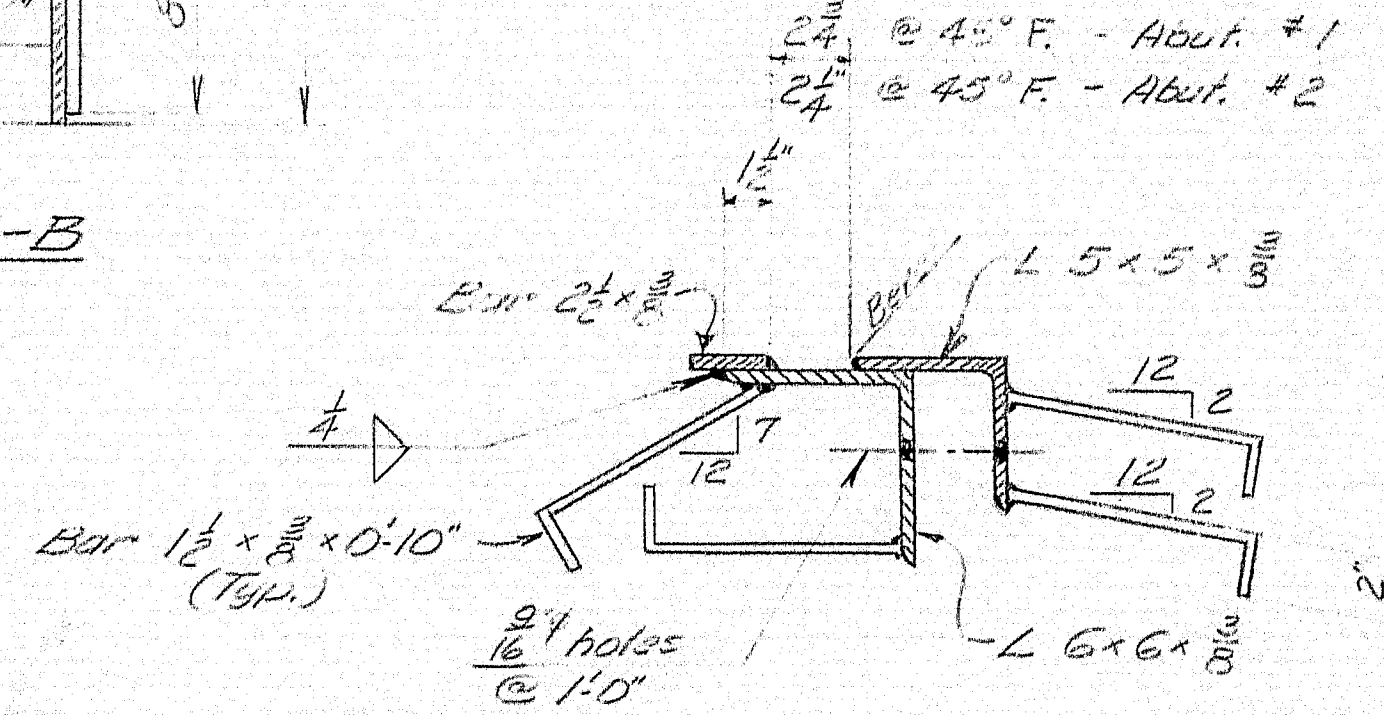
SECTION A-A



PLAN



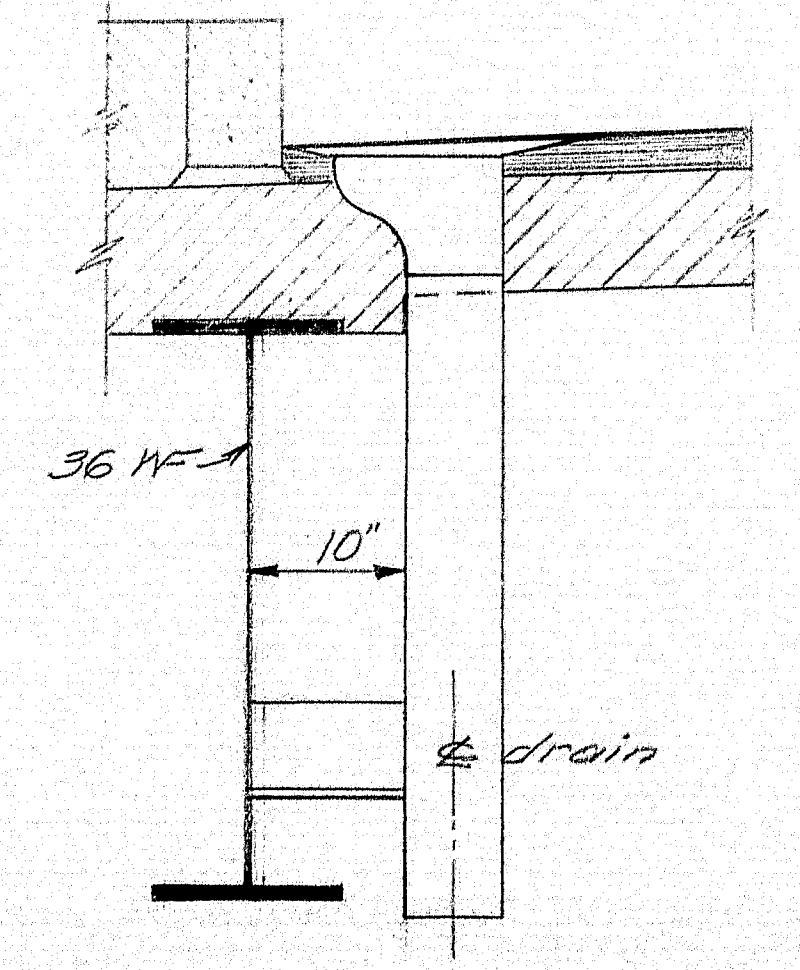
SECTION B-B



SECTION C-C

CURB DAM

- 2- Curb Dams at each abutment
- 4- Required
- 2- as shown and 2- opposite hand



DRAIN

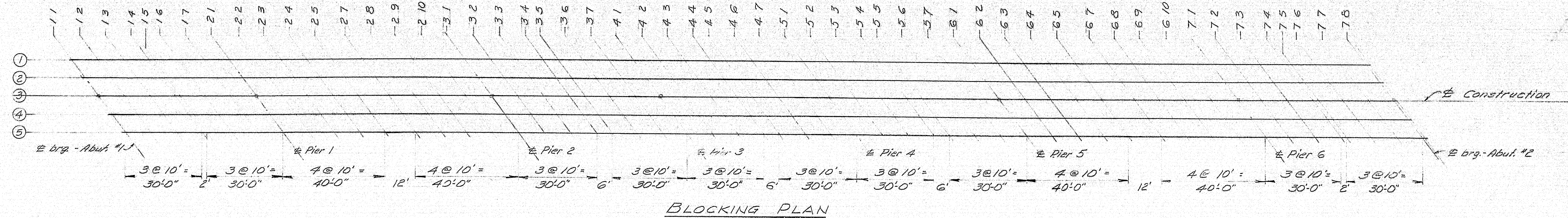
20-drains required
NOTE: For location see Superstructure Sheet 14.
See Standard Details sheet BD 104-62

EXPANSION DAM

- 1- Expansion Dam at each abutment
- 2- Required

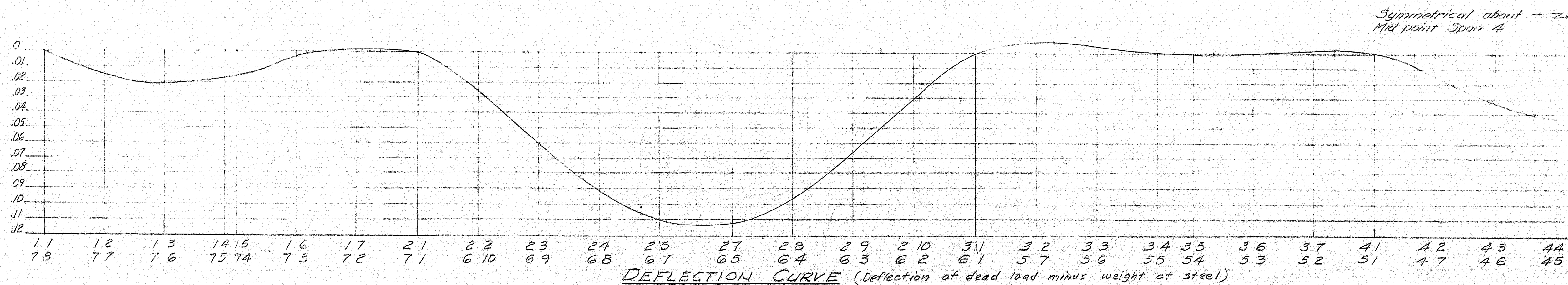
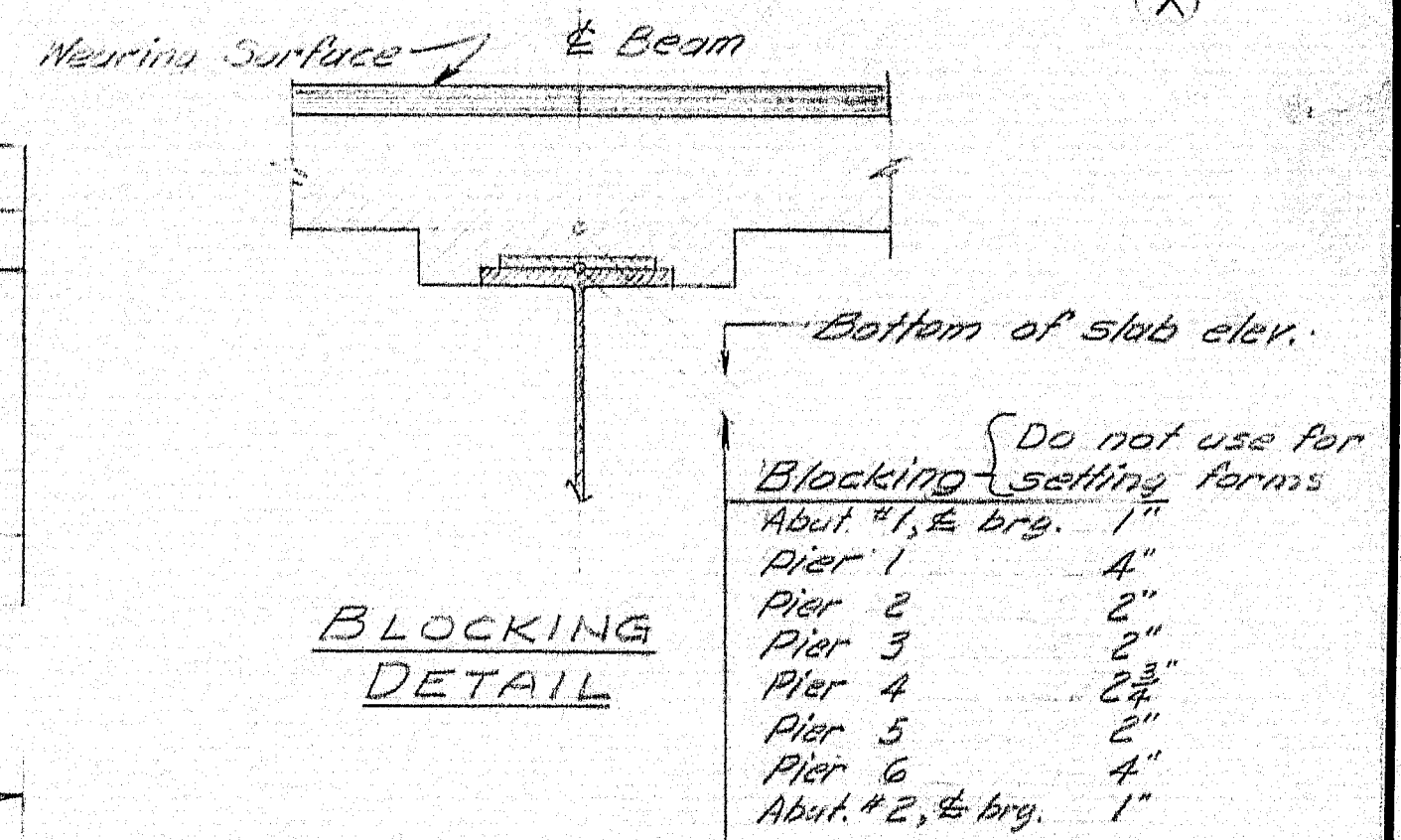
DESIGN- C.D.H.	BRIDGE NO.
TRACE- G.M.C.	SURVEY- PLOT-
CHECK- T.H.R.	

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
ROUTE 16
OVER
INTERSTATE 95
IN THE CITY OF
OLD TOWN
PENOBSCOT COUNTY
STRUCTURAL STEEL - DETAILS
SHEET // OF 17 AUGUSTA, MAINE JAN. 1964



BOTTOM OF SLAB ELEVATIONS																														
POINT	11	12	13	14	15	16	17	21	22	23	24	25	27	28	29	210	31	32	33	34	35	36	37	41	42	43	44	45	46	47
①	148.29	148.37	148.45	148.51	148.52	148.56	148.62	148.67	148.74	148.81	148.88	148.93	148.97	148.98	148.97	148.95	148.93	148.93	148.93	148.94	148.94	148.93	148.92	148.90	148.89	148.88	148.86	148.84	148.80	148.74
②	148.47	148.55	148.63	148.69	148.70	148.74	148.79	148.84	148.91	148.98	149.05	149.10	149.13	149.14	149.13	149.10	149.08	149.08	149.08	149.09	149.08	149.07	149.05	149.04	149.02	149.01	148.99	148.97	148.92	148.86
③	148.66	148.73	148.81	148.87	148.88	148.92	148.97	149.01	149.06	149.15	149.21	149.26	149.29	149.29	149.26	149.23	149.23	149.23	149.24	149.22	149.21	149.19	149.17	149.15	149.14	149.12	149.09	149.04	148.98	
④	148.55	148.62	148.70	148.75	148.76	148.80	148.84	148.89	148.96	149.02	149.08	149.12	149.15	149.15	149.14	149.11	149.09	149.08	149.08	149.07	149.07	149.05	149.03	149.01	148.99	148.98	148.95	148.92	148.88	148.81
⑤	148.44	148.51	148.58	148.63	148.64	148.68	148.72	148.76	148.83	148.89	148.95	148.99	149.01	149.01	149.00	148.97	148.94	148.93	148.93	148.92	148.91	148.90	148.87	148.85	148.83	148.81	148.78	148.76	148.70	148.63

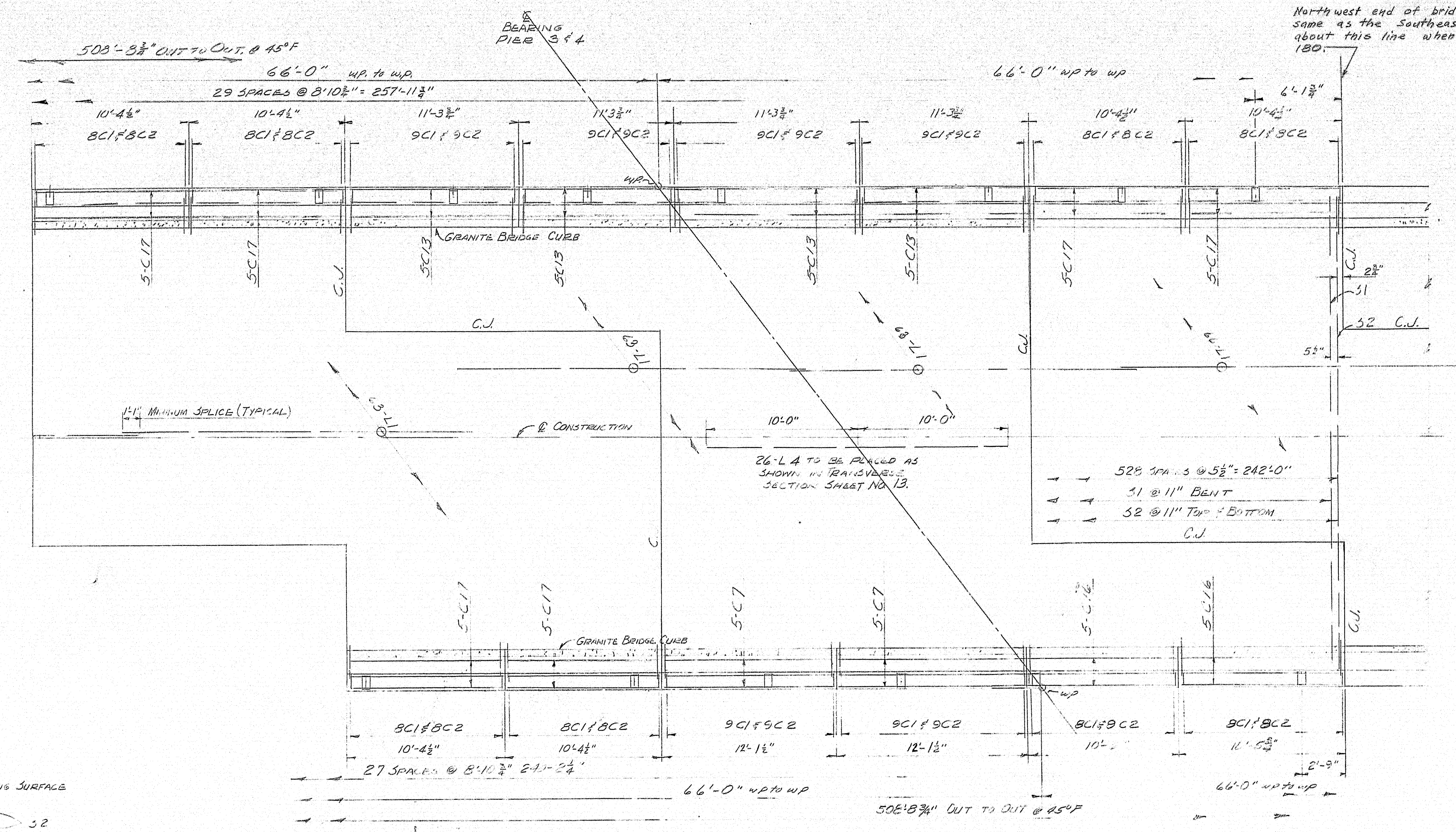
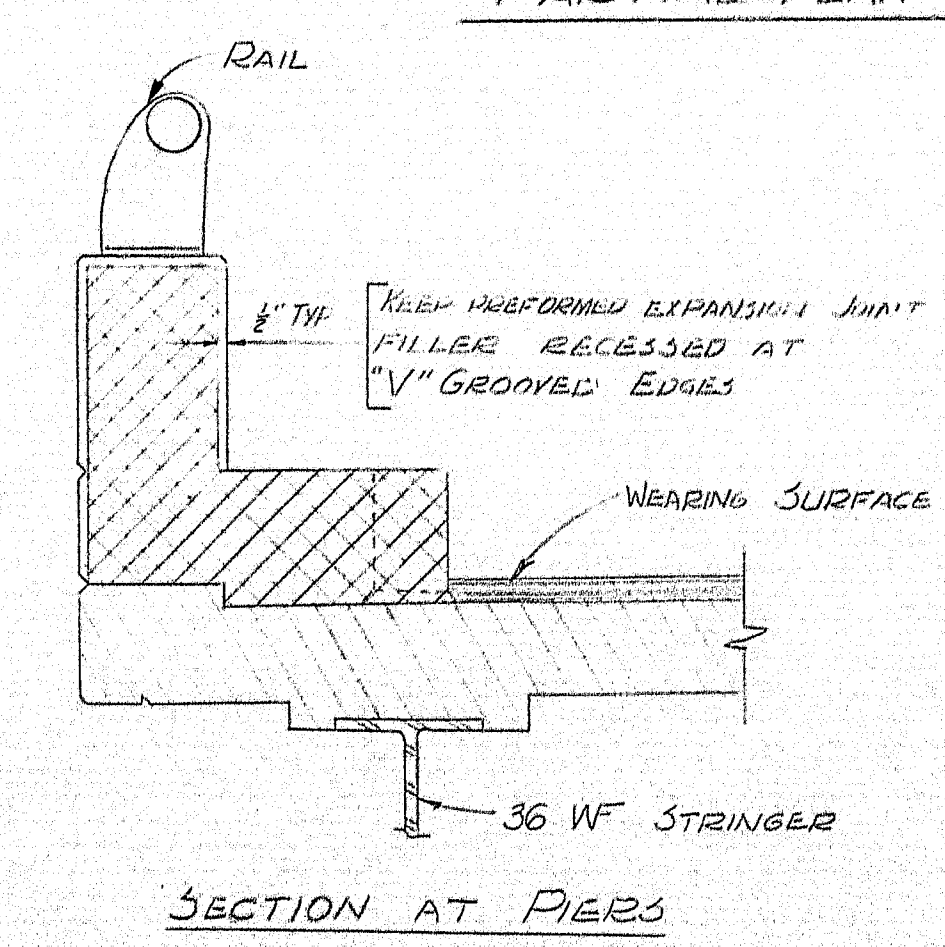
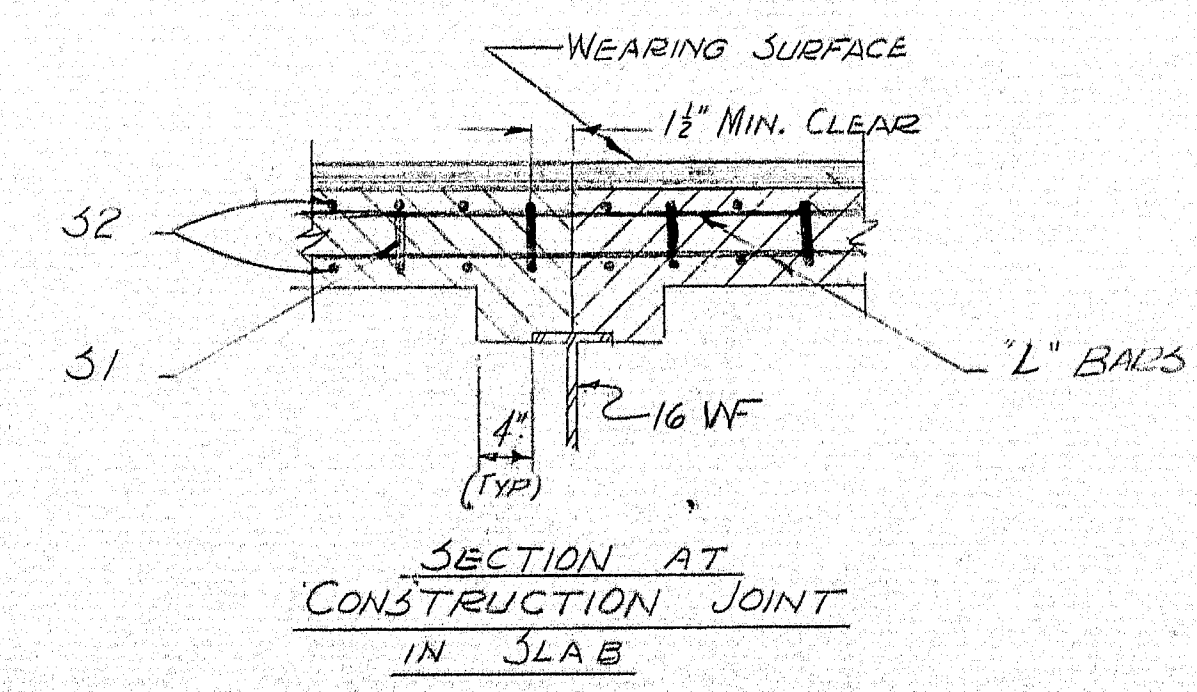
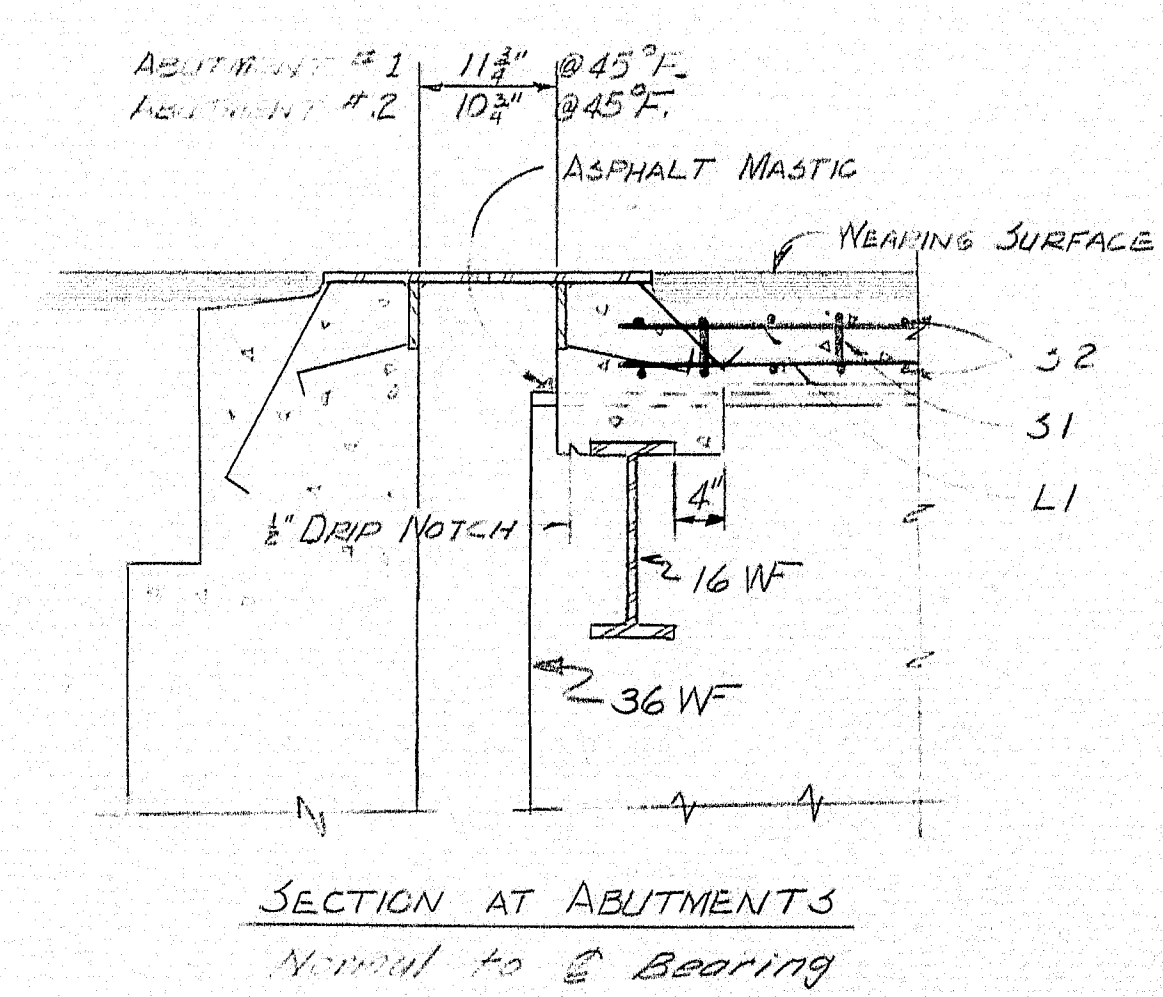
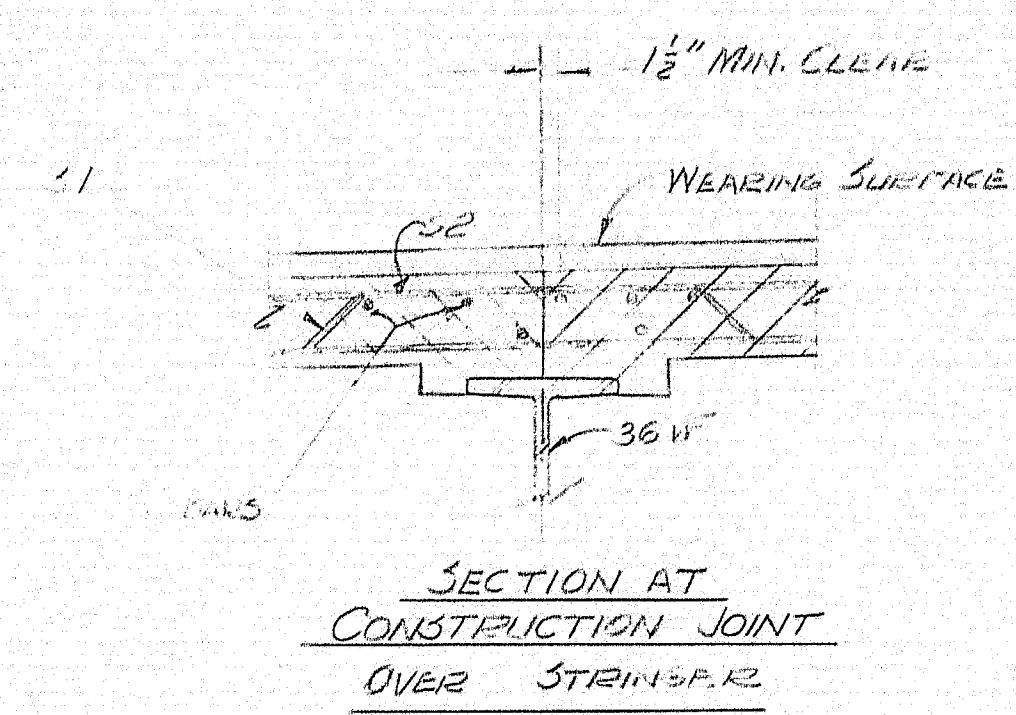
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NOTE:
In order that the wearing surface may conform to the profile and cross sections shown on these plans, the accompanying table of "Bottom of Slab Elevations" are given. These elevations which are computed to compensate for dead load deflections must be set before slab forms are to be started.

B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-8(39)		

Northwest end of bridge is the same as the Southeast end about this line when rotated 180°.



DESIGN - CDH
TRACE - JMK
CHECK - THK

BRIDGE NO.
SURVEY -
PLOT -

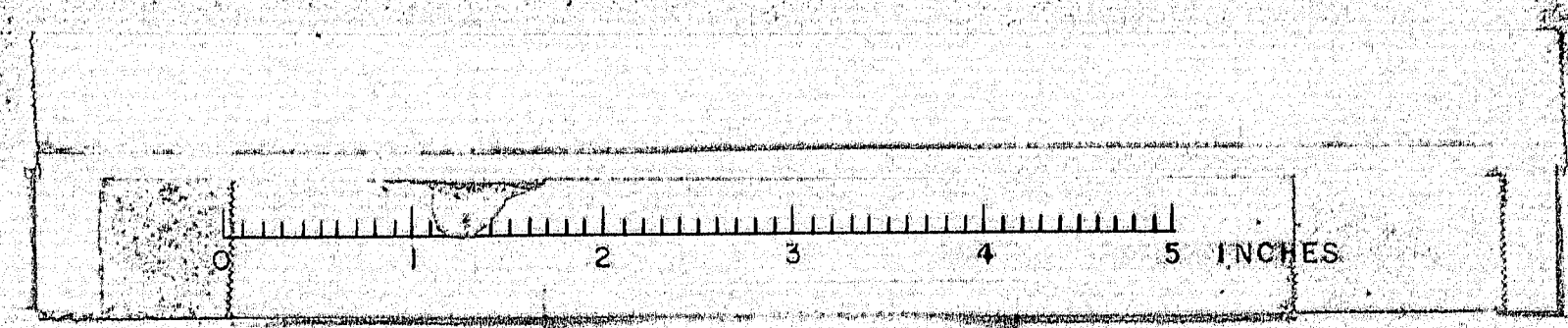
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

ROUTE 16
OVER
INTERSTATE 95
IN THE CITY OF
OLD TOWN
PENOBSCOT COUNTY

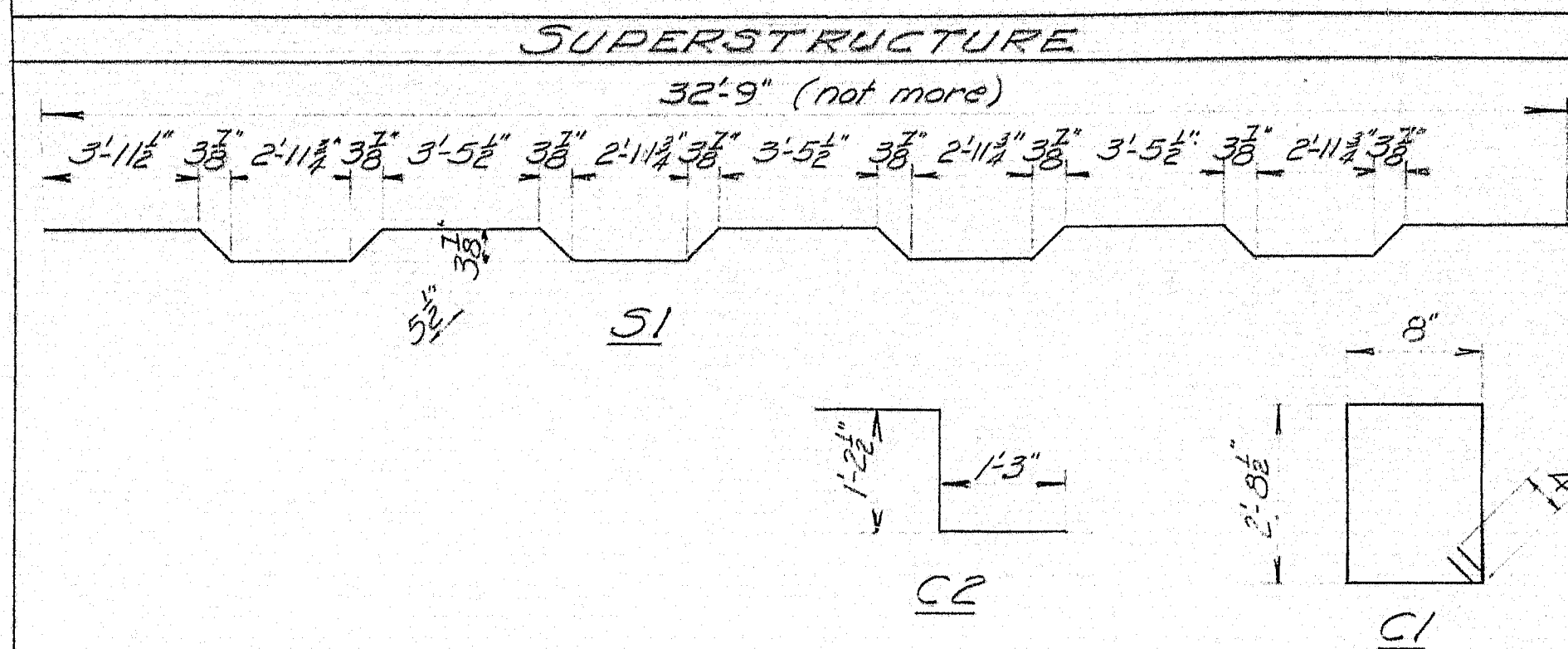
SUPERSTRUCTURE - SPAN 3, 4 & 5

SHEET 15 OF 17 AUGUSTA, MAINE JAN. 1964

92-99



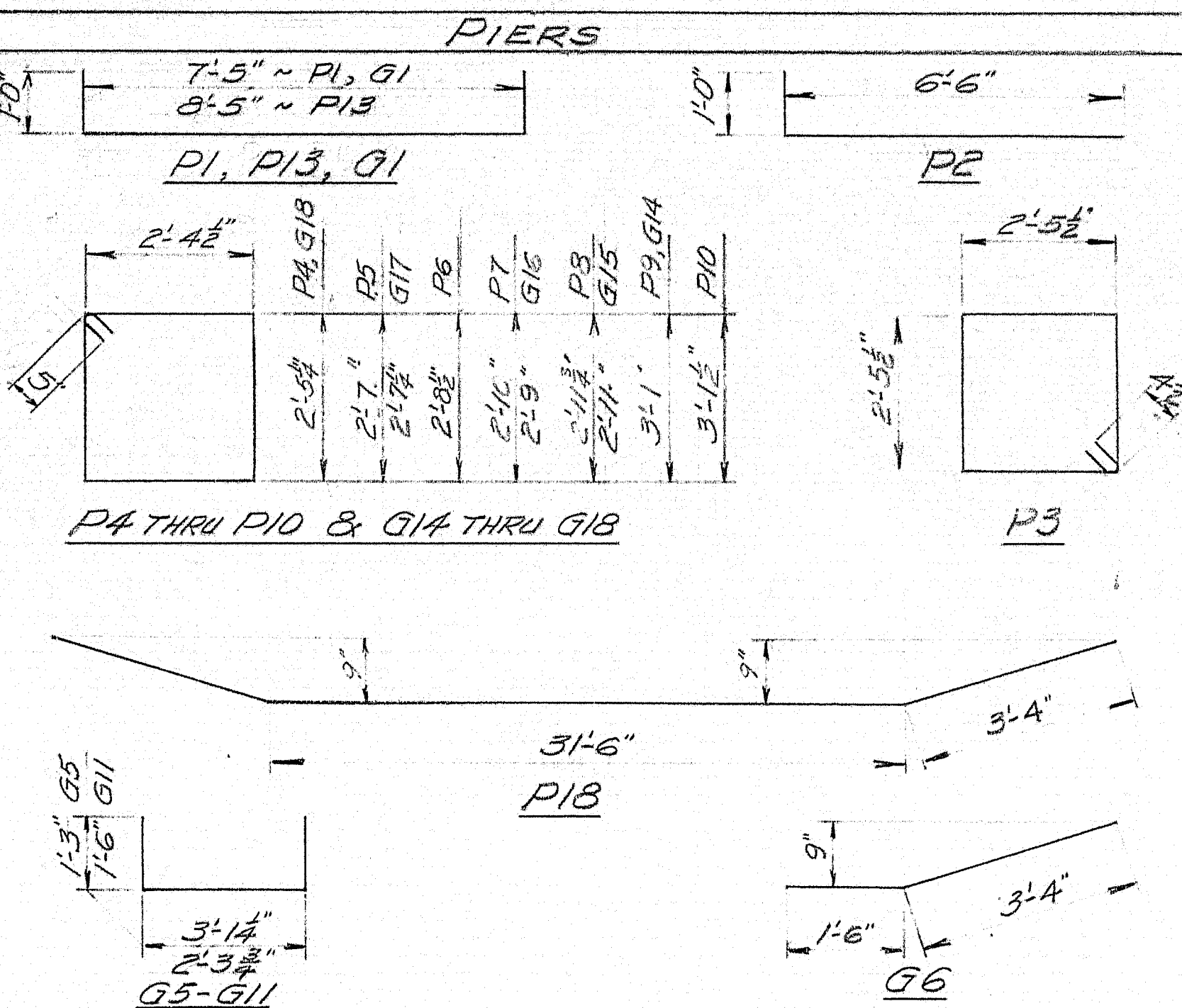
REINFORCING STEEL SCHEDULE

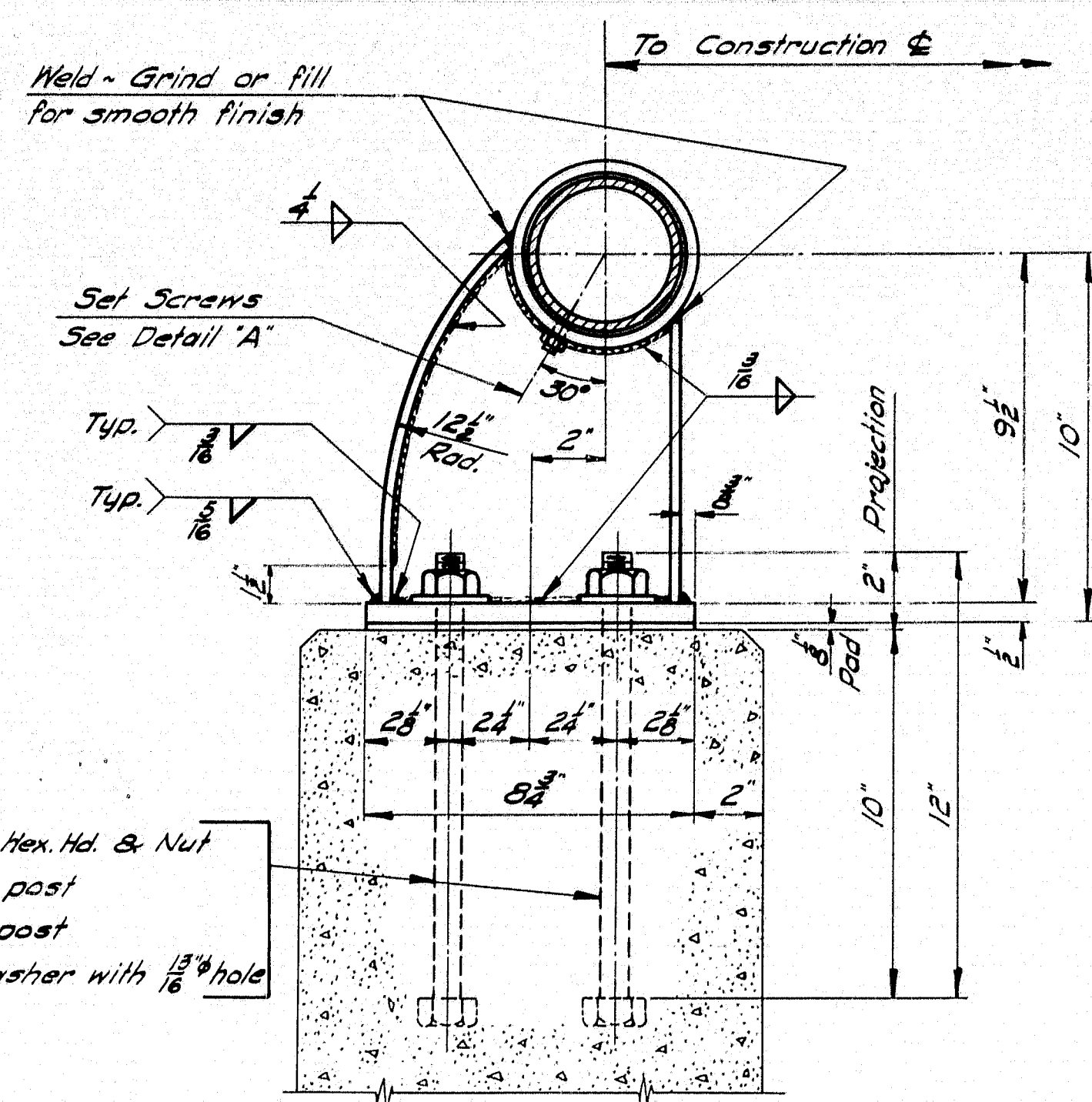


BENT BARS				
MARK	SIZE	No.	LENGTH	LOCATION
C1	#4	778	7'-5"	Curb
C2	#4	778	3'-9"	Curb
C3	#5	528	33'-10"	Slab - Main Steel

STRAIGHT BARS				
MARK	SIZE	No.	LENGTH	LOCATION
C3	#4	30	16'-3"	Curb & Parapet
C4	#4	30	15'-3"	
C5	#4	30	14'-7"	
C6	#4	30	13'-2"	
C7	#4	20	11'-9"	
C8	#4	20	11'-8"	
C9	#4	20	11'-6"	

MARK	SIZE	No.	LENGTH	LOCATION
C11	#4	40	11'-5"	
C12	#4	20	11'-0"	
C13	#4	20	10'-11"	
C14	#4	20	11'-2"	
C15	#4	20	10'-3"	
C16	#4	20	10'-1"	
C17	#4	20	10'-0"	
S1	#5	1060	32'-9"	Slab - Main Steel
S2	#5	1060	29'-6"	
S3	#5	1060	28'-4"	
S4	#5	1060	27'-1"	
S5	#5	1060	25'-10"	
S6	#5	1060	24'-8"	
S7	#5	1060	23'-5"	
S8	#5	1060	22'-3"	
S9	#5	1060	21'-0"	
S10	#5	1060	19'-10"	
S11	#5	1060	18'-7"	
S12	#5	1060	17'-5"	
S13	#5	1060	16'-3"	
S14	#5	1060	15'-0"	
S15	#5	1060	13'-7"	
S16	#5	1060	12'-7"	
S17	#5	1060	11'-4"	
S18	#5	1060	10'-1"	
S19	#5	1060	8'-11"	
S20	#5	1060	7'-9"	
S21	#5	1060	6'-6"	
S22	#5	1060	5'-3"	
S23	#5	1060	4'-1"	
S24	#5	1060	2'-10"	
S25	#5	1060	10'-0"	
L1	#5	1449	23'-2"	Slab Distribution Steel
L2	#5	52	24'-0"	
L3	#5	52	23'-0"	
L4	#5	52	20'-0"	

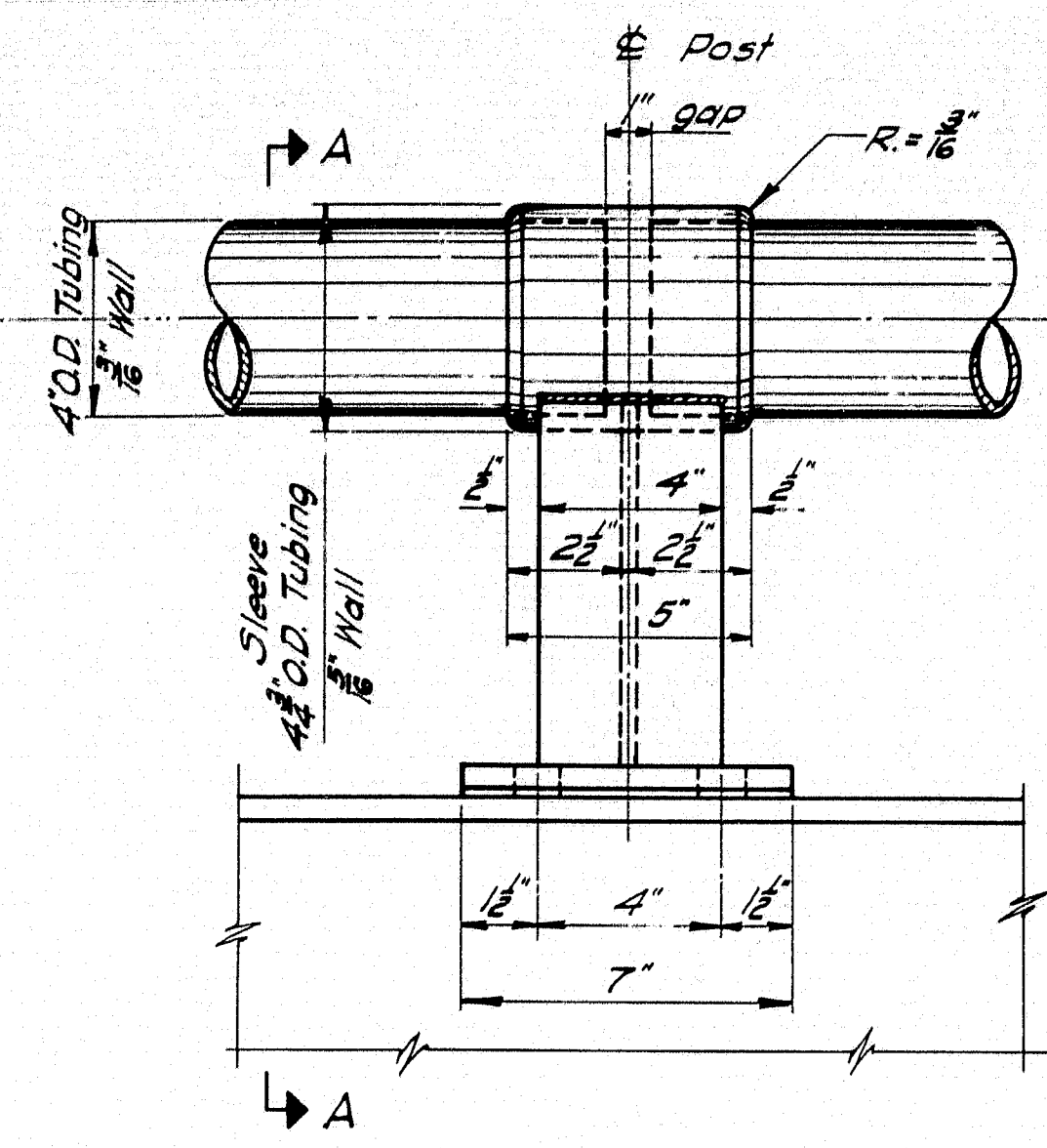




SECTION A-A

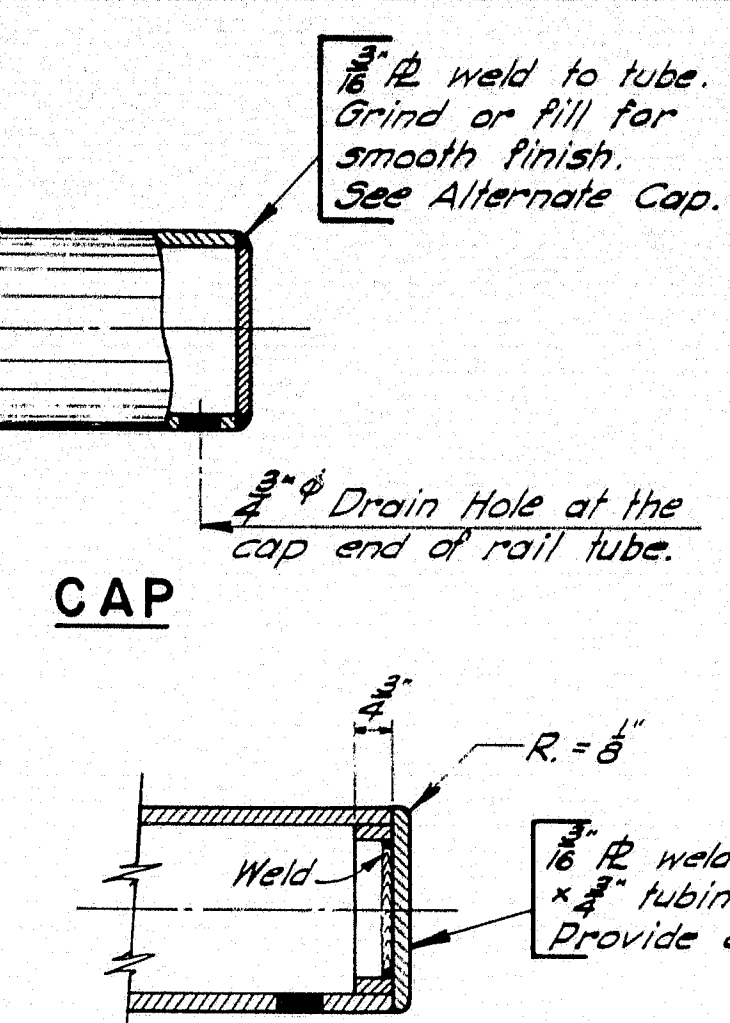
Post shown to be fabricated from 8" x 4" x 1/8" WF welded to 7" x 1/2" x 1/8" base plate and 4" O.D. 1/8" wall x 0.5" tube or sleeve.

An alternate cast steel post may be used if the outside dimensions conform to the dimensions shown as detailed, and is approved by the Engineer.

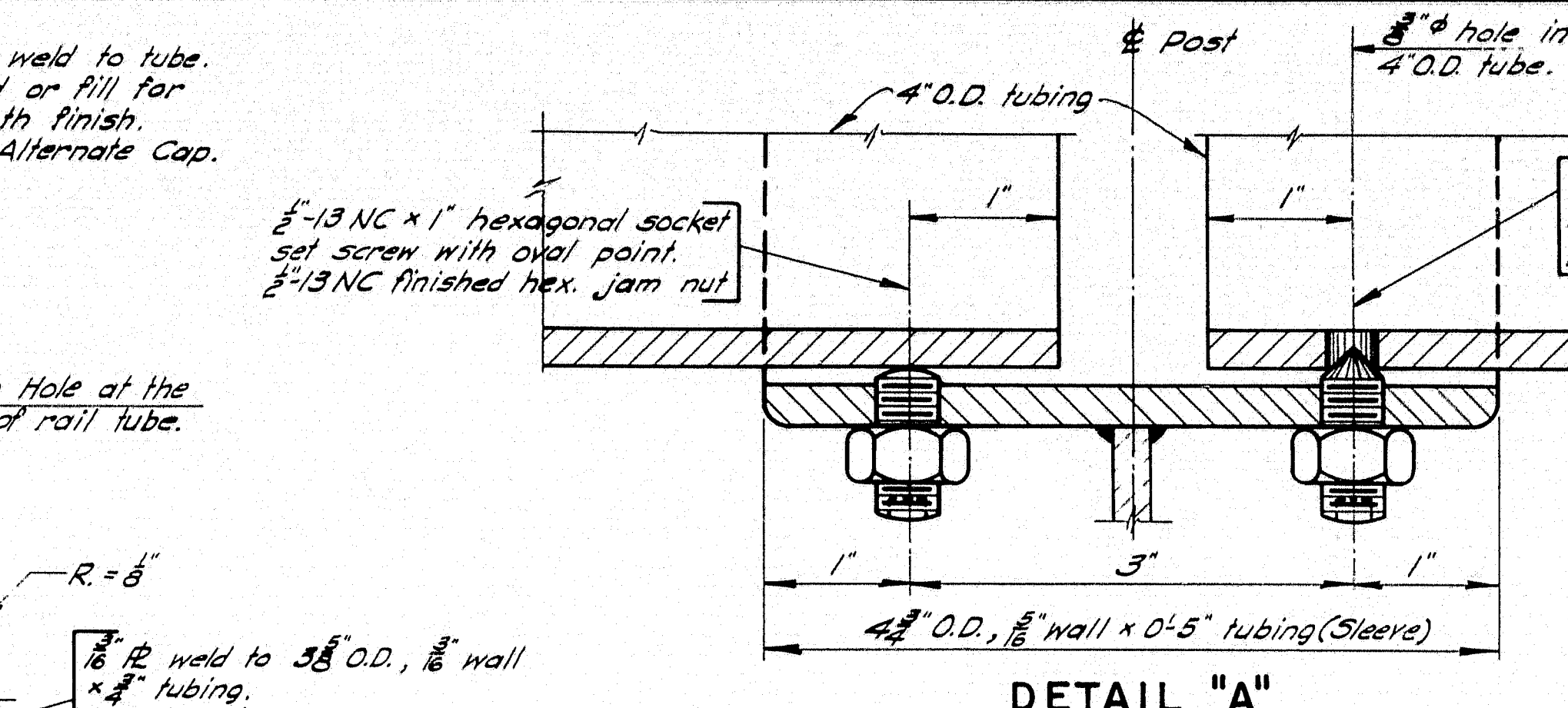


RAIL POST

4" O.D. tubing to be continuous for a minimum of two rail panels. For tubing two or more panels long the cone point set screw to be tight in the hole provided in 4" O.D. tubing. At all other posts one oval point set screw per post shall be set to bear. Where tubes are not continuous thru posts leave 1" gap at the center of post.

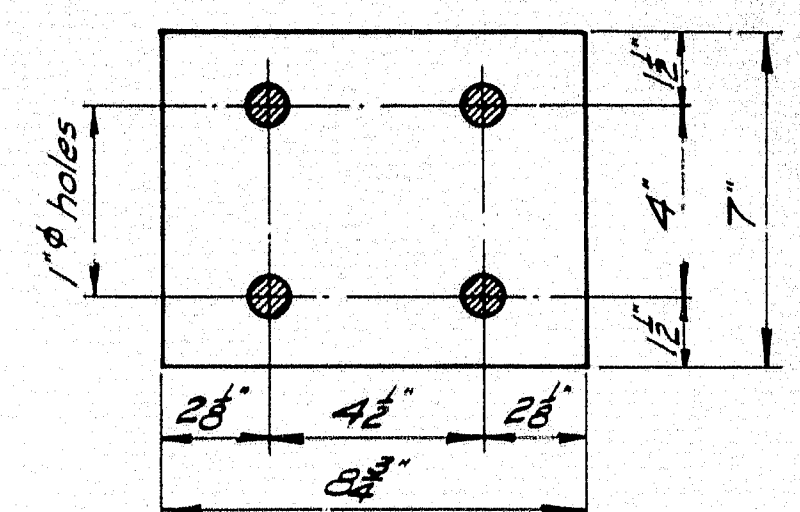


ALTERNATE CAP



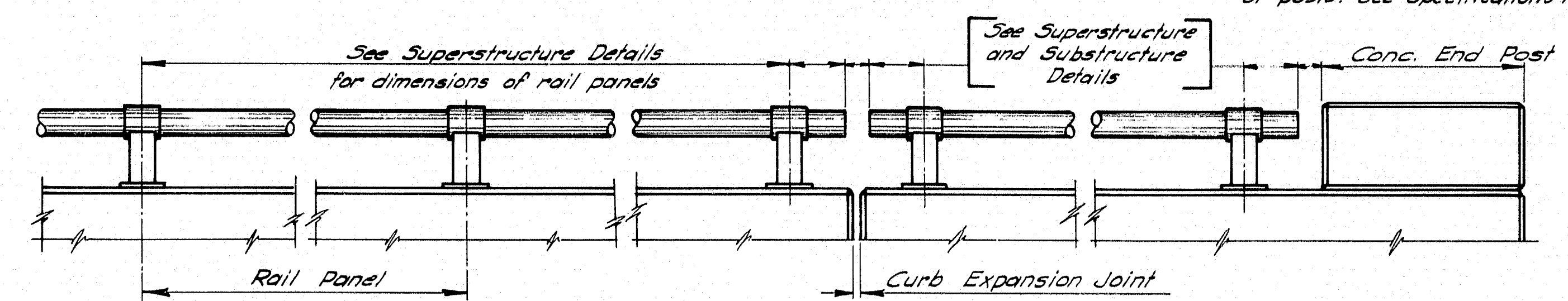
DETAIL "A"

Drill and tap sleeve 1/2"-13 NC (2-holes). Drill 4" O.D. tubing (Rail) 3/8" hole, 1" from one end only.



PAD

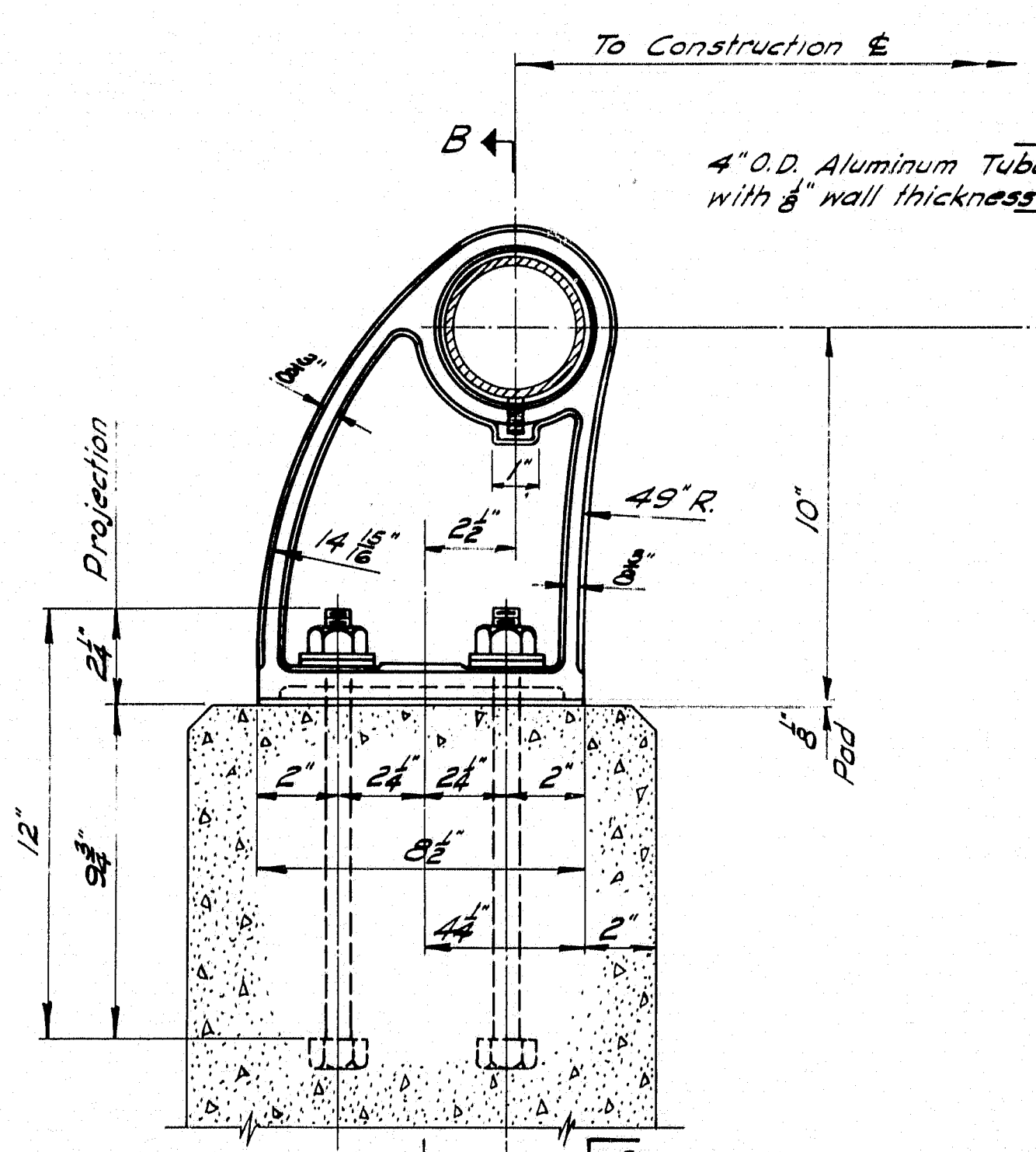
At least one pad shall be placed under each post, and the number of pads supplied shall be 10% in excess of total number of posts. See Specifications for material.



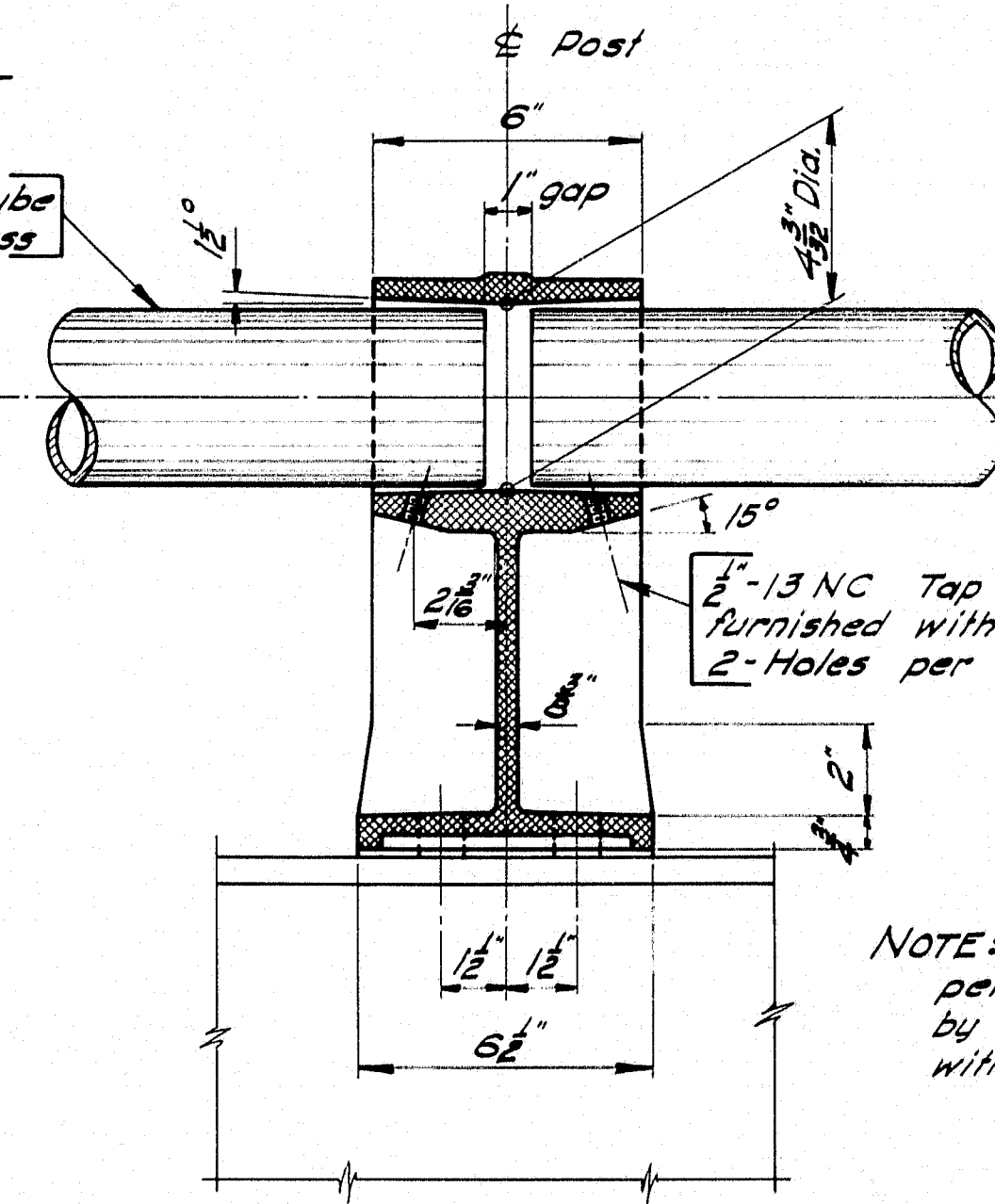
SIDE ELEVATION - RAIL

NOTE: 4" O.D. Tubing, Posts, Bolts, Nuts & Washers to be galvanized.

ONE BAR RAIL - STEEL



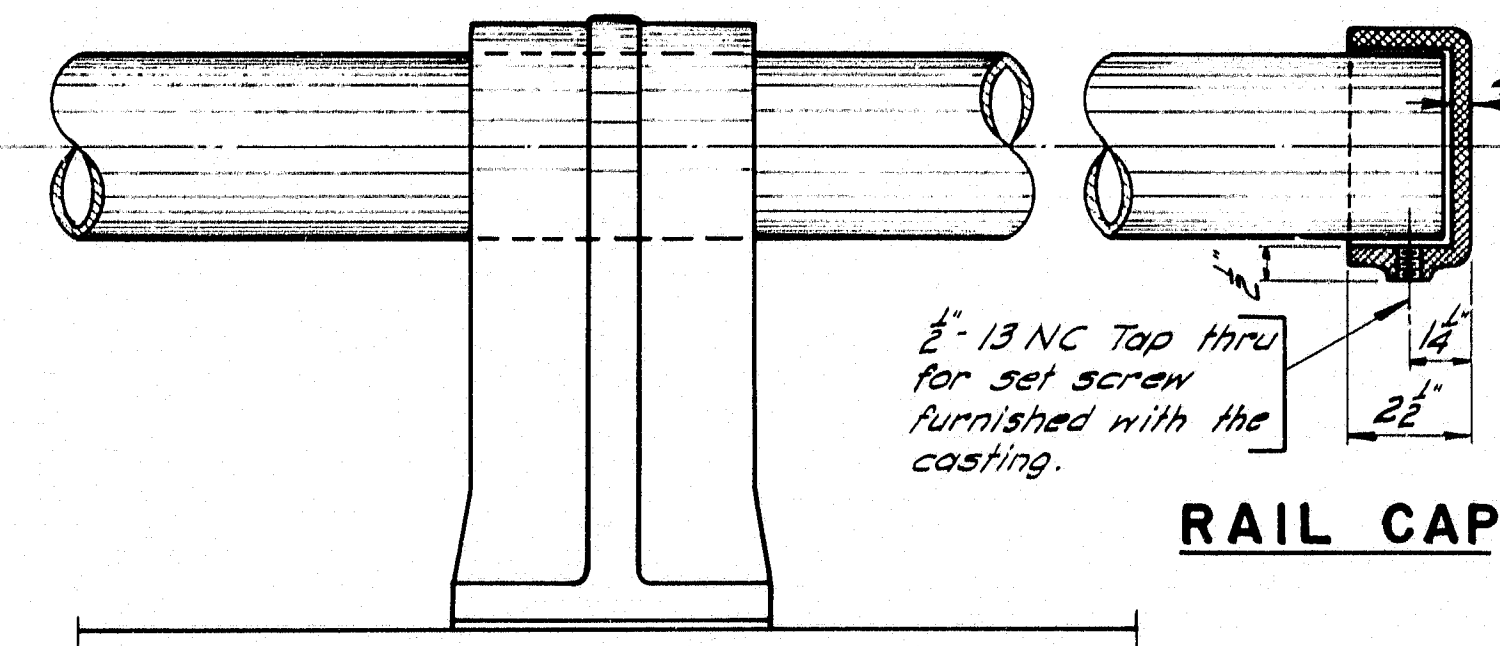
RAIL POST



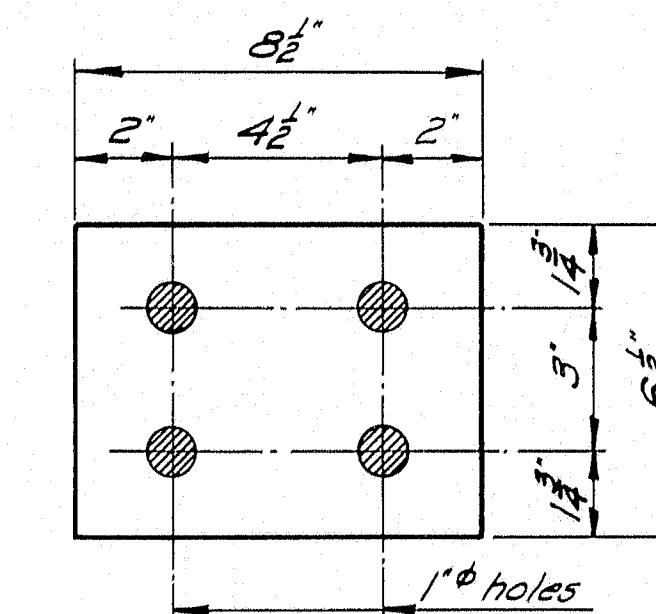
SECTION B-B

NOTE: Slight variations will be permitted in details, if approved by the Engineer, in order to conform with various manufacturers castings.

NOTE: Aluminum tubes to be continuous for a minimum of two rail panels. For tubes two or more panels long set screws to be tight at one interior post and to bear at all other posts. Where tubes are not continuous thru posts leave a 1" gap at the center of post.

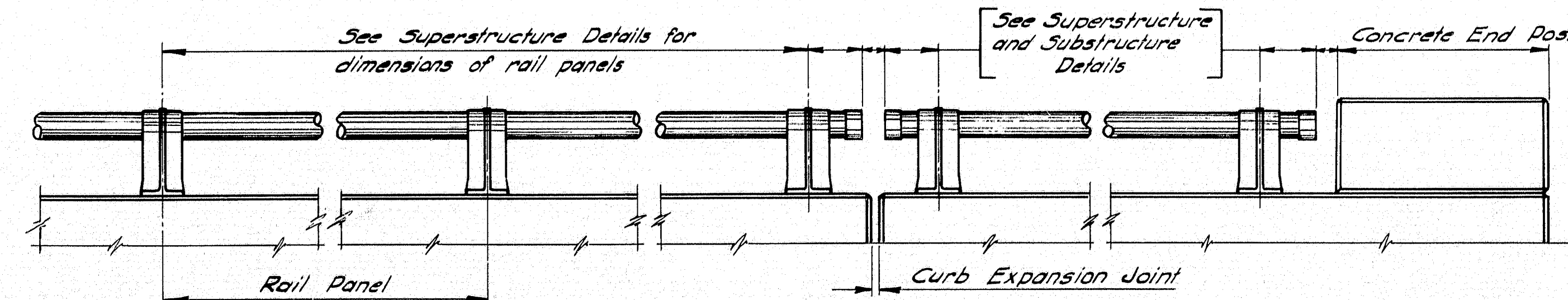


RAIL CAP



PAD

At least one pad shall be placed under each post, and the number of pads supplied shall be 10% in excess of total number of posts. See Specifications for material.



SIDE ELEVATION - RAIL

ONE BAR RAIL - ALUMINUM

GENERAL NOTES

RAIL ON CURVE

If the curve radius is more than 150 feet and is less than 2700 feet, the rail tube is to be straight and one panel length only, unless otherwise called for in the design details. For curves with a radius less than 150 feet see design details. For curves with a radius more than 2700 feet rail details shall be as shown on this sheet.

Design details govern if there is conflict between dimensions shown in design details and the standards.

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

STANDARD DETAILS
(BD 102-62)

BRIDGE RAIL

ONE BAR RAIL - STEEL
ONE BAR RAIL - ALUMINUM

NOV. 1962

92-100B

